

# Elephant & Castle Station – Heritage Opportunities

November 19 2025

Benedict O’Looney Architects

Richmond Station – Southern Railway Architects 1935 – Restored front elevation







## Richmond Station – Southern Railway Architects 1935 – Restored front bronze canopy





## Richmond Station – Southern Railway Architects 1935 – Stair detailing





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EXIT

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## Richmond Station – Southern Railway Architects 1935





## Richmond Station – Southern Railway Architects 1935



## Richmond Station – Southern Railway Architects 1935





Richmond Station – Southern Railway Architects 1935 – Stair detailing



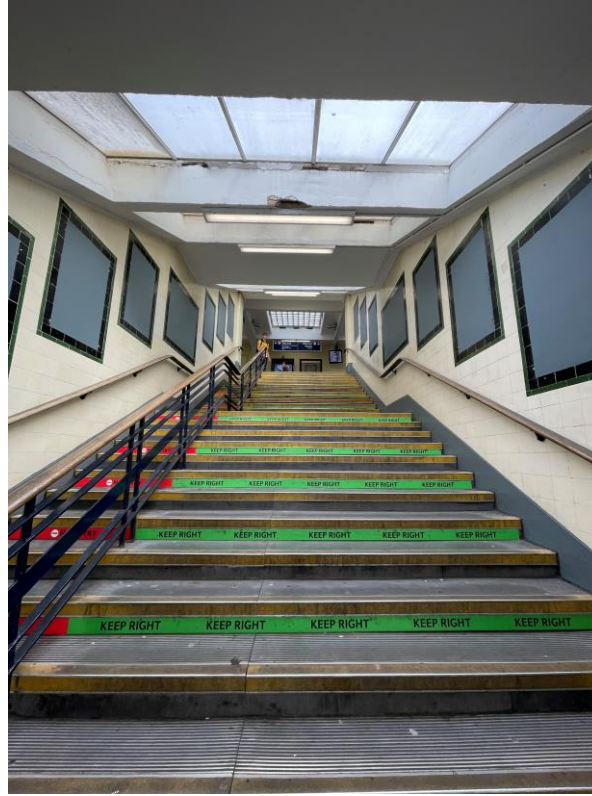
## Richmond Station – Southern Railway Architects 1935 – passenger bridge



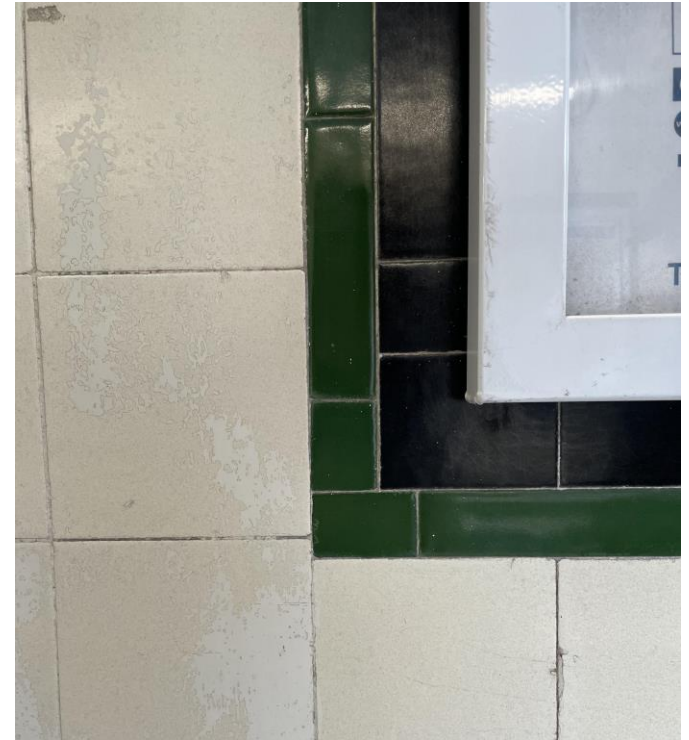


# Surbiton Station – Southern Railway Architects 1935

## Stairs to the platforms



Richmond Station – Southern Railway Architects 1935 – Using period posters in the restored tiled poster surrounds





Richmond Station – Southern Railway Architects 1935 – Using period posters in the restored tiled poster surrounds

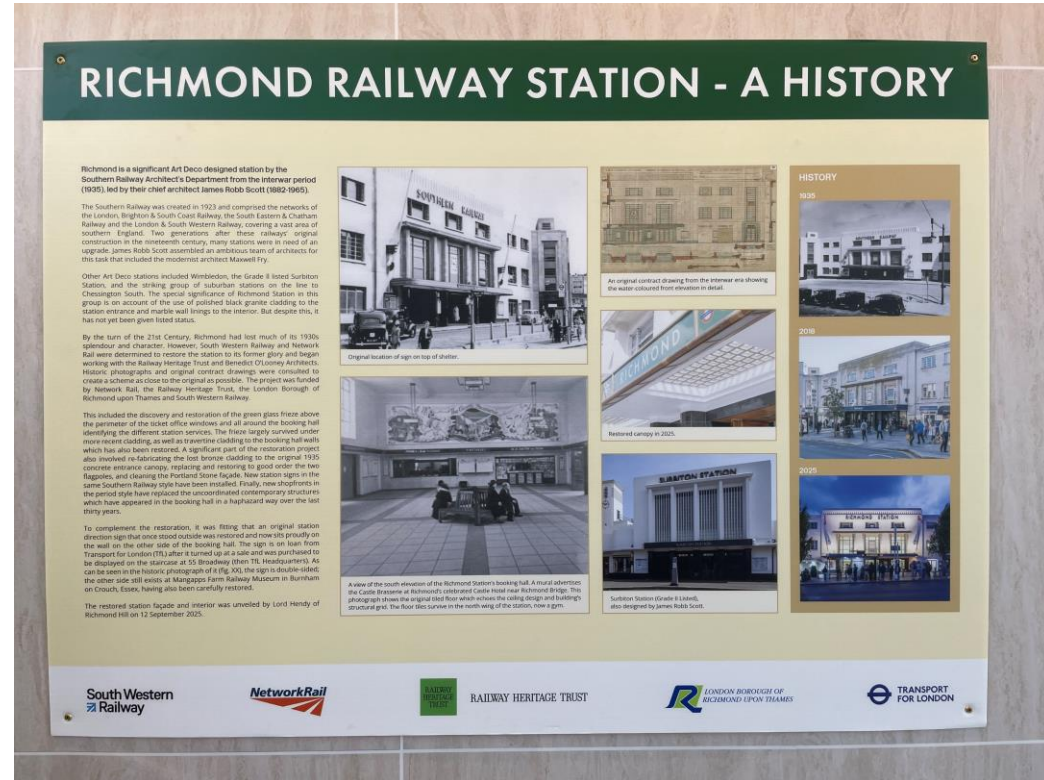


Richmond Station Booking Hall – Southern Railway Architects, 1935 – Using period posters with black tile and travertine surrounds – Summer 2025



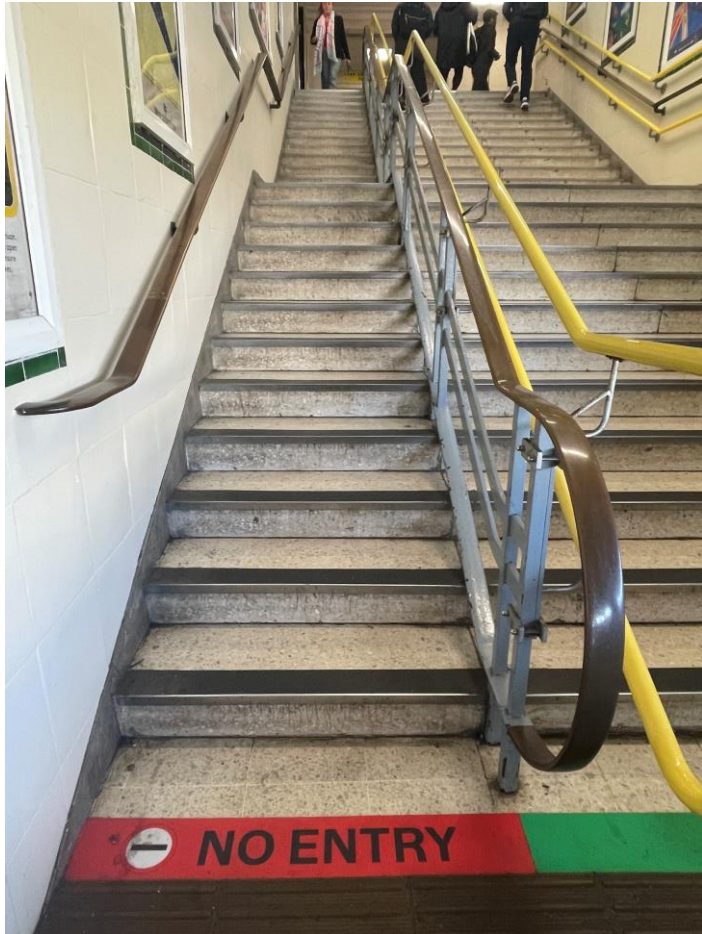


# Richmond Station – restoration



# Richmond Station – restoration

## Richmond Station – Southern Railway Architects 1935 – Stair detailing





# SURBITON STATION

ION CARS

STATION CARS

SURBITON











## Southern Railway Architects

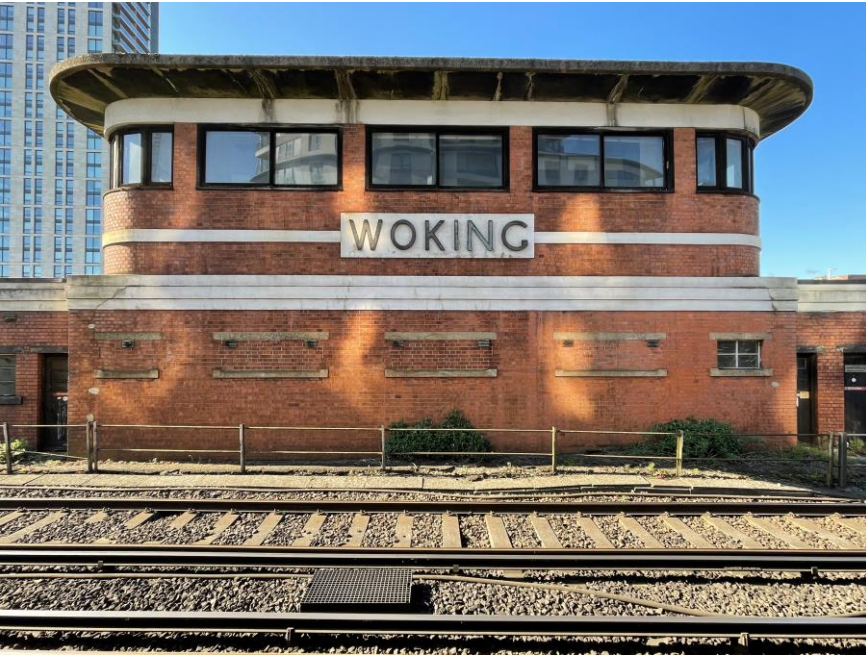






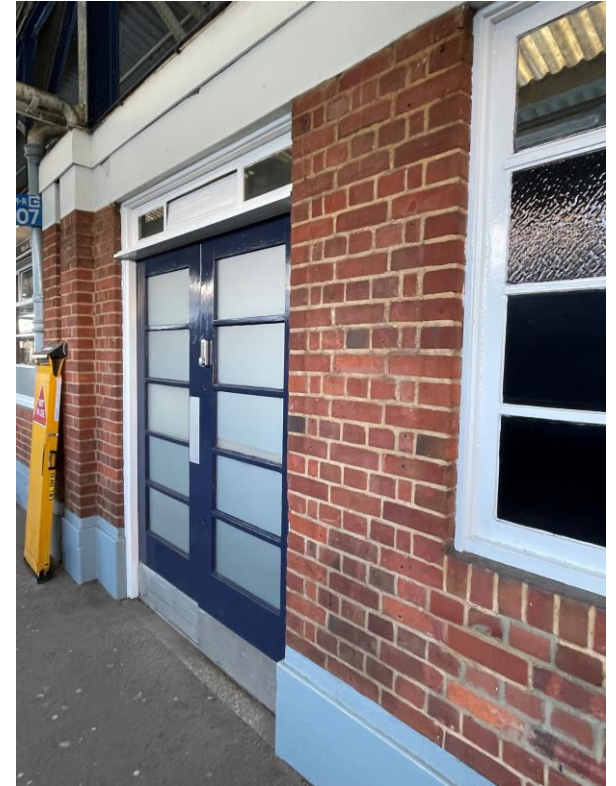


## Woking Station – Southern Railway Architects





## Woking Station – Southern Railway Architects







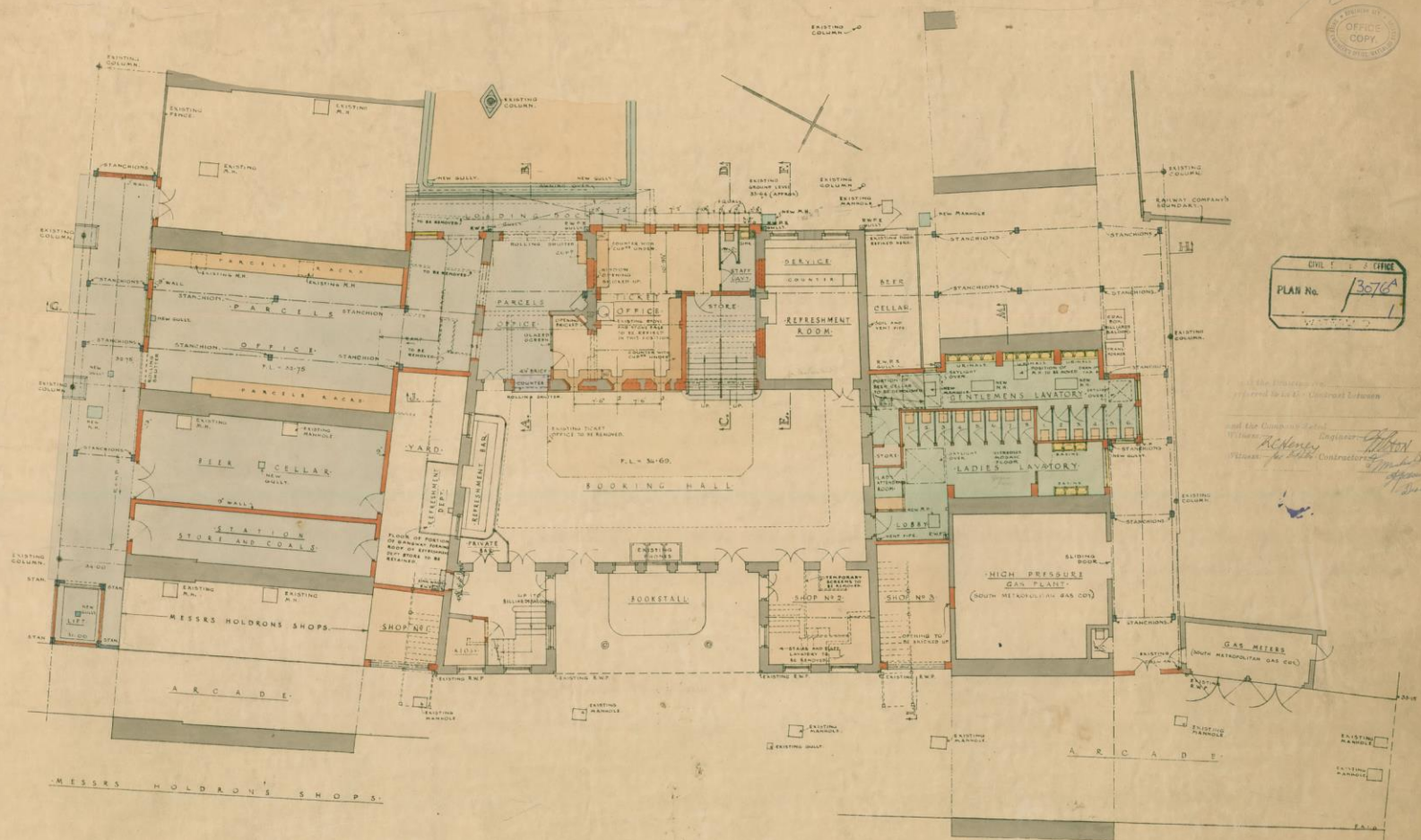




# S. R. PECKHAM RYE STATION IMPROVEMENTS.

NOTE FOR DETAILS OF STEELWORK  
SEE ENGINEERS DRAWINGS.

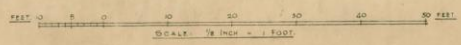
SELF ENGINEER'S OFFICE  
PLAN No. 11/3076  
15  
WATERLOO  
A BRANCH OF THE  
OFFICE  
COPY



CIVIL ENGINEER  
PLAN No. 11/3076  
15

The Engineer has  
prepared this plan in accordance with the  
instructions of the  
Messrs. R. & S. Peckham  
Messrs. R. & S. Peckham  
Messrs. R. & S. Peckham

GROUND FLOOR PLAN.



PAVEMENT LEVEL AT  
ARCADE ENTRANCE = 31.0

copy to L.S. 24-6-35.  
- Mr. Engineer 9-8-35.  
- Mr. Cunningham (undated) 9-8-35.  
- Mr. 6-12-35.

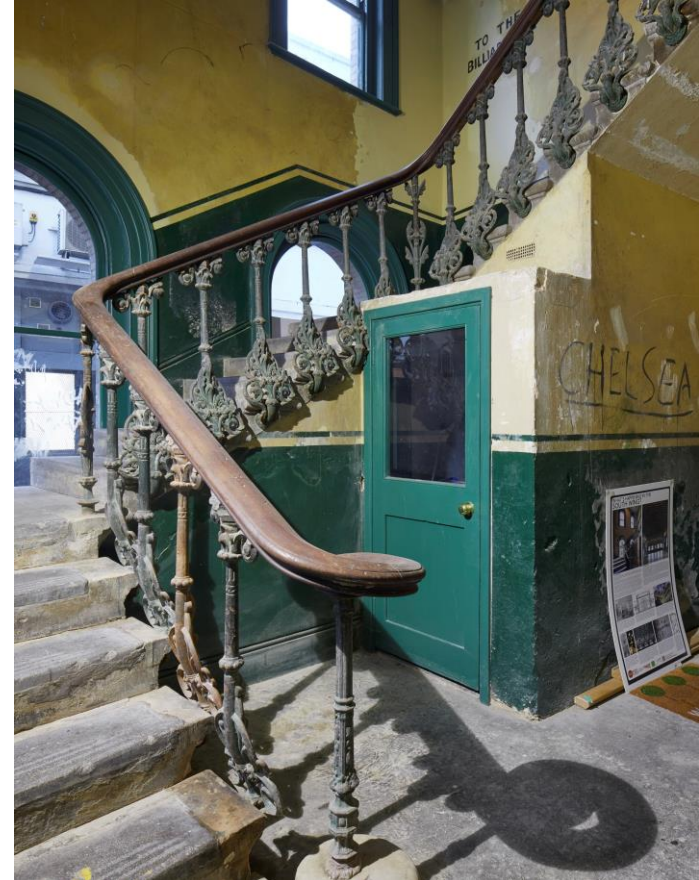
NEG. NO. 25,748

Peckham Rye– Southern Railway Architects circa 1935





Elephant & Castle Station – Southern Railway Architects circa 1930  
and 1950





## Richmond Station – Southern Railway Architects 1935 – Stair detailing





## Elephant & Castle Station – Southern Railway Architects



# Elephant & Castle Station – Southern Railway Architects

TOT. WHITE GLASS. BIG  
DEN TACA. CLAMPING AGAINST  
CAST IRON SKELETON. PALACE  
OF WESTMINSTER. FALCON  
BIRDS OF PREY. MIKE. M. CANN  
THE KEEPER OF THE CLOCK.  
MARK COLLINS HISTORIAN AND  
LIBRARIAN.

RE-DISCOVER  
THE HIDDEN  
1930'S TILED POSTER  
PANEL SURROUNDS  
NOW HIDDEN

WORRIED ABOUT WATER?  
A GANT MATWEIL. ABOUT  
HAWK.

FLOOR  
CONSIDER A NEW FLOOR  
FINISH. THE PRESENT SURFACE  
IS LIFE - EXPIRED.

NICE DRAWING OF  
ENTRANCE NR. 25030  
HORIZONTAL SHUTTLE. NICE  
NICE DETAILS TOO.

1388. PLATFORM CANOPY DRAWINGS  
S. R. WATERLOO STATION. JULY 1947  
E. L. W. M. C. C. H. I. App. Arch.  
OCTOBER 1947.

TOM AND BARBARA. THE GOOD LIFE. THE

KENDALL FAMILY. INDIA.

CORTES AND PISSARO. 16" C.  
CHARLES (N) AND ISABELLA. FAST. FIRM LOVE



REMOVE MODERN  
OVER CLADDING  
WHY?

IN RICHMOND.  
THE LOWER TIER  
IS GRANITE. HIGH QUALITY  
MATERIALS.

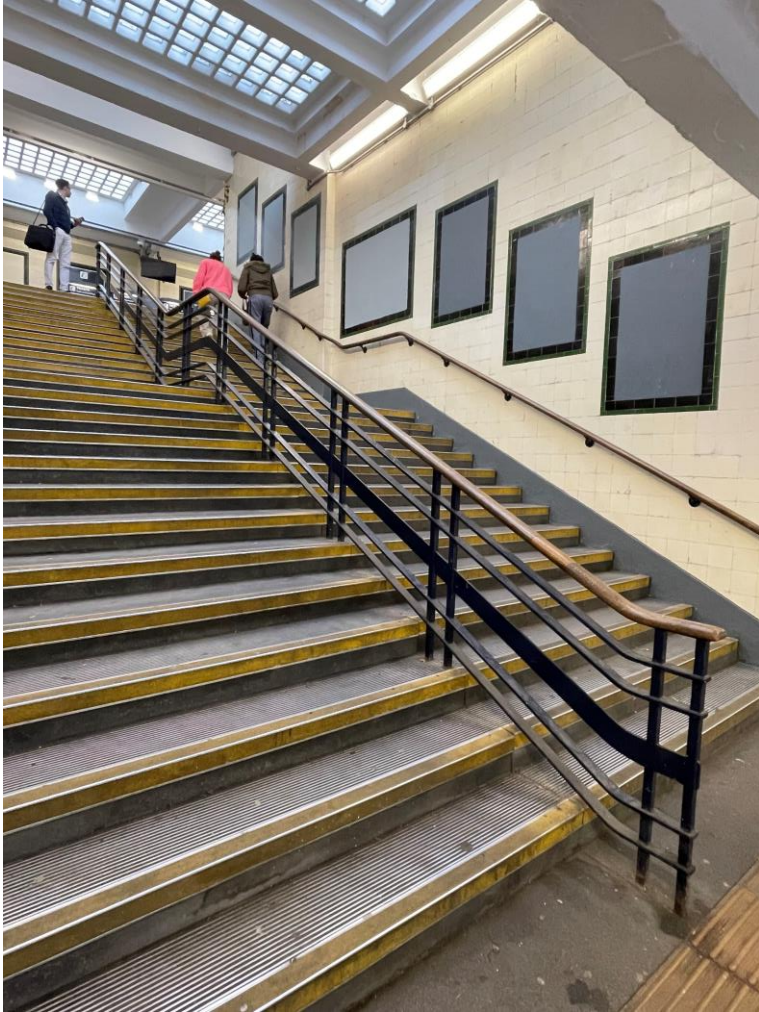
CLEAN THE MODERN PAINT  
OF THE 1930'S FEATURE STAIR.

INVESTIGATE UNDER THE PRESENT  
FLOOR FINISH.

GRANITE FLOOR AND 6" GRANO SKIRTING (1949. DRAWING. G. E. WALLIS + SONS  
THE RAILWAY EXECUTIVE.  
4" 0" TO ONE INCH  
29 SEPTEMBER 1949) 30 SEP. 1949 WA



## Surbiton Station – Southern Railway Architects 1935 – Stair detailing





cts





A THOUGHTFUL  
TWO-TONE  
PAINT SCHEME  
COULD SUBTLY  
ACCENTUATE THE  
PROGRESSIVE FERRO  
CONCRETE CONSTRUCTION  
OF THE STATION

REVEAL THE  
MID-20<sup>th</sup> CENTURY  
POSTER PANEL TILED  
SURROUNDS WITH GREEN  
AND BLACK TILES

CONSIDER A TWO  
TONE PAINT SCHEME  
PERHAPS THE MID-  
20<sup>th</sup> CENTURY PAINT  
SCHEME.

# Elephant & Castle Station



Existing Plan



Aspect 0 - Ticket Office Relocation and Demolition of Town Centre Entrance Box



Aspect 1 - Installation of Town Centre Temporary Entrance and Gatelines



Aspect 2 - Station Enhancement Scheme



Network Rail Colour Code	
(Red)	New equipment / items
(Black)	Existing equipment / items to be retained
(Green)	Existing equipment / items to be removed
(Blue)	Modified equipment / items
(Purple)	Temporary works
(Grey)	OS / Survey data

- Drawing Notes**
1. Do not scale from this drawing.
  2. This drawing shall be read in conjunction with all specifications, architectural, civil, structural, mechanical and electrical drawings.
  3. Figured dimensions only to be used.
  4. All dimensions are in millimetres unless otherwise stated.
  5. All dimensions to be checked on site before commencement of works.
  6. Any differences or discrepancies must be referred to the contract.

HD	411125	Preliminary Issue	DR	AT	CL
REV	1/25	Preliminary Issue	DR	AT	CL
Rev	Date	Description of Revisions	Drawn	Chkd	Appr
Status	Preliminary Issue				



Project  
**Elephant & Castle  
Accessibility Improvements**

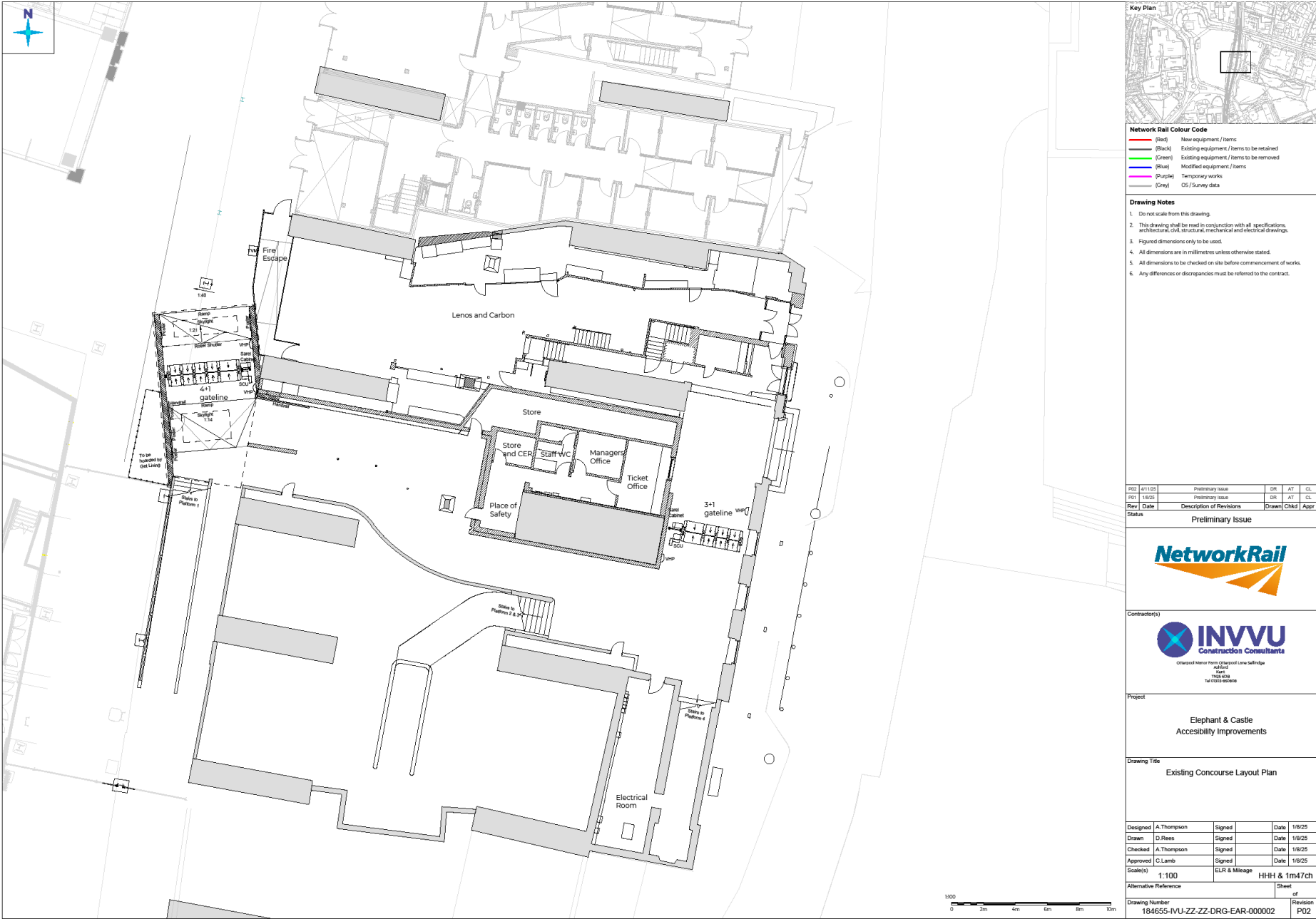
Drawing Title  
**Station Improvements Phasing**

Designed	A.Thompson	Signed		Date	1/6/25
Drawn	D.Rees	Signed		Date	1/6/25
Checked	A.Thompson	Signed		Date	1/6/25
Approved	C.Lamb	Signed		Date	1/6/25
Scale(s)	1:200	ELR & Mileage	HHH & 1m47ch		

Alternative Reference	Sheet	of	Revision
Drawing Number 184655-IVU-ZZ-ZZ-DRG-EAR-000001			P02



# Elephant & Castle Station



Network Rail Colour Code	
(Red)	New equipment / items
(Black)	Existing equipment / items to be retained
(Green)	Existing equipment / items to be removed
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  4. All dimensions are in millimetres unless otherwise stated.
  5. All dimensions to be checked on site before commencement of works.
  6. Any differences or discrepancies must be referred to the contract.

Rev	Date	Description of Revisions	Drawn	Checkd	Appr
PGD	4/11/25	Preliminary Issue	DR	AT	CL
PGI	18/05	Preliminary Issue	DR	AT	CL

Status: Preliminary Issue



Project: Elephant & Castle Accessibility Improvements

Drawing Title: Existing Concourse Layout Plan

Designed	A. Thompson	Signed		Date	18/25
Drawn	D. Rees	Signed		Date	18/25
Checked	A. Thompson	Signed		Date	18/25
Approved	C. Lamb	Signed		Date	18/25

Scale(s): 1:100  
Alternative Reference: HHH & 1m47ch  
Drawing Number: 184655-IVU-ZZ-ZZ-DRG-EAR-000002  
Revision: P02

# Elephant & Castle Station



**Network Rail Colour Code**

(Red)	New equipment / items
(Black)	Existing equipment / items to be retained
(Green)	Existing equipment / items to be removed
(Blue)	Modified equipment / items
(Purple)	Temporary works
(Grey)	OS / Survey data

- Drawing Notes**
1. Do not scale from this drawing.
  2. This drawing shall be read in conjunction with all specifications, architectural, civil, structural, mechanical and electrical drawings.
  3. Figured dimensions only to be used.
  4. All dimensions are in millimetres unless otherwise stated.
  5. All dimensions to be checked on site before commencement of works.
  6. Any differences or discrepancies must be referred to the contract.

P02	4/11/25	Preliminary Issue	DR	AT	CL
P01	1/8/25	Preliminary Issue	DR	AT	CL
Rev	Date	Description of Revisions	Drawn	Chkd	Appr
Status					

Status: Preliminary Issue



Project: Elephant & Castle Accessibility Improvements

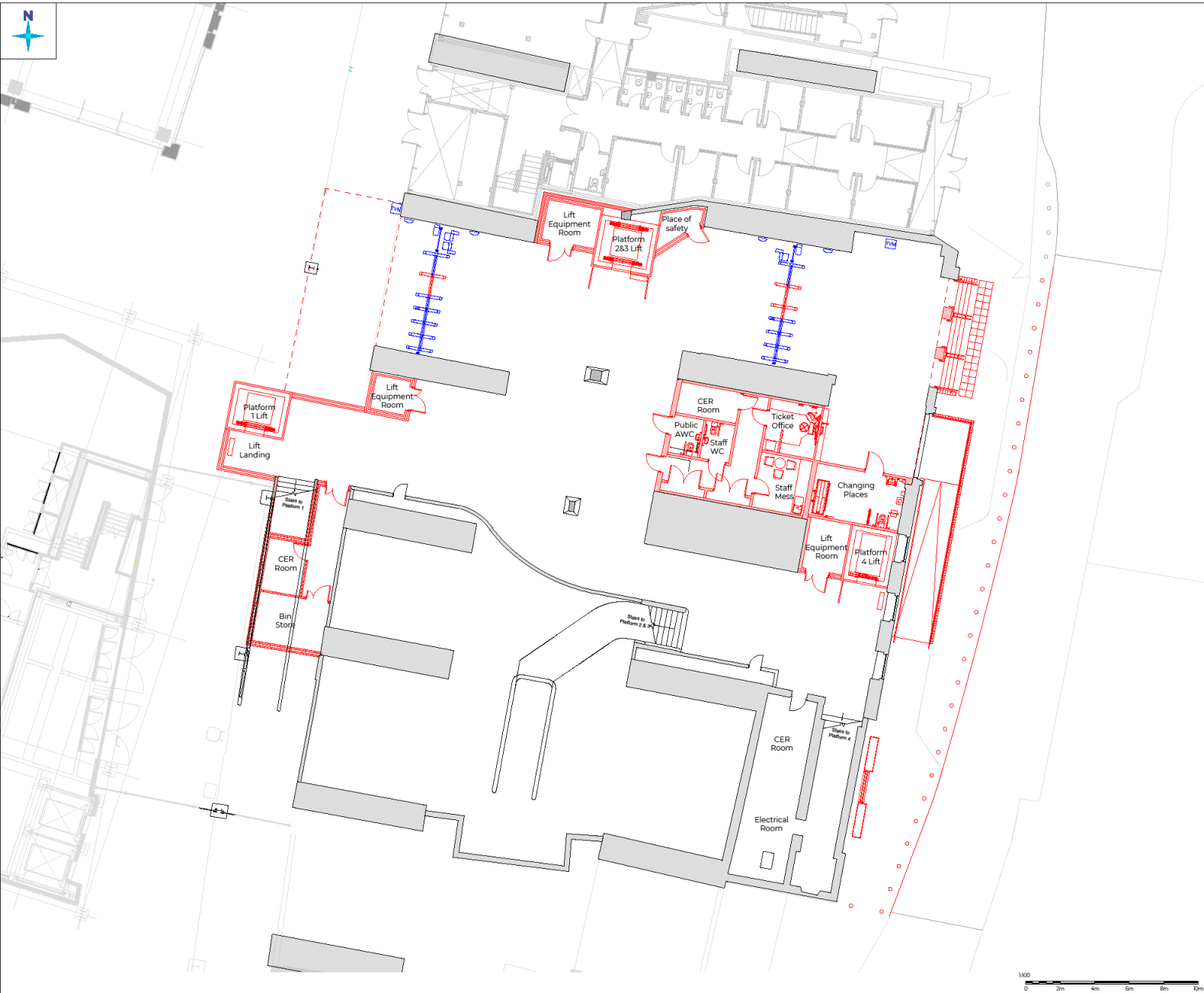
Drawing Title: Concourse Demolition Plan

Designed	A. Thompson	Signed		Date	1/8/25
Drawn	D. Rees	Signed		Date	1/8/25
Checked	A. Thompson	Signed		Date	1/8/25
Approved	C. Lamb	Signed		Date	1/8/25
Scale(s)	1:100	ELR & Mileage	HHH & 1m47ch		

Alternative Reference	Sheet	Revision
Drawing Number 184655-IVU-ZZ-ZZ-DRG-EAR-000003		P02



# Elephant & Castle Station



Network Rail Colour Code	
(Red)	New equipment / items
(Black)	Existing equipment / items to be retained
(Green)	Existing equipment / items to be removed
(Blue)	Modified equipment / items
(Purple)	Temporary works
(Grey)	OS / Survey data

- Drawing Notes**
1. Do not scale from this drawing.
  2. This drawing shall be read in conjunction with all specifications, architectural, civil, structural, mechanical and electrical drawings.
  3. Figured dimensions only to be used.
  4. All dimensions are in millimetres unless otherwise stated.
  5. All dimensions to be checked on site before commencement of works.
  6. Any differences or discrepancies must be referred to the contract.

P02	6/1/2025	Preliminary Issue	DR	AT	CL
P01	1/8/25	Preliminary Issue	DR	AT	CL
Rev	Date	Description of Revisions	Drawn	Chkd	Appr

Status: Preliminary Issue



Project: Elephant & Castle Accessibility Improvements

Drawing Title: Proposed Concourse Layout Plan

Designed	A.Thompson	Signed		Date	1/8/25
Drawn	D.Rees	Signed		Date	1/8/25
Checked	A.Thompson	Signed		Date	1/8/25
Approved	C.Lamb	Signed		Date	1/8/25
Scale(s)	1:100	ELR & Mileage	HHH & 1m47ch		

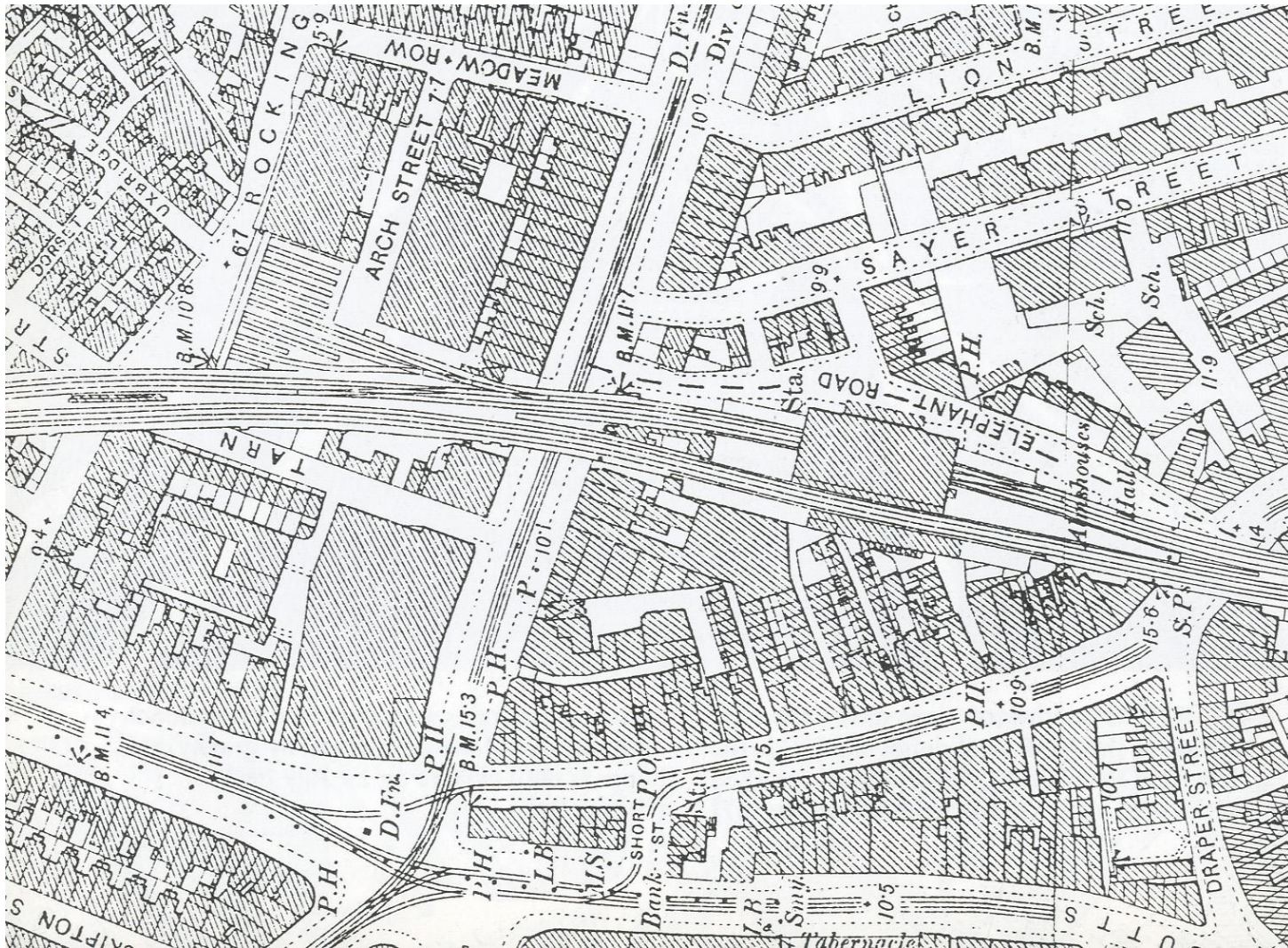
Drawing Number	184655-IVU-ZZ-ZZ-DRG-EAR-000004	Revision	P02
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# Elephant & Castle Station





## Elephant & Castle Station – 1895











In the days when these buildings were covered with a century's dirt, it is perhaps not surprising that they were not highly regarded either by the public or by railway management. Little sense of bright colour is evident in this late 1960s view of Herne Hill! The SR advertising continues to stress the twin aims of the LC&DR's Metropolitan Extensions. *Jim Connor*

Herne Hill c.1900, showing the south end of the building, which was not given any great architectural treatment, save for the bands. At first glance the 1862 LC&DR valancing of the roadside canopy might be taken as the standard SER 1890s/1900s type.

*J.E. Connor collection*



This illustration from *The Building News* 9.1.1863 is the only known view of any of the stations when new, and shows how the original 'Taylor' tiles (for which the architect was well-known) particularly added distinction to the roofs here, more so perhaps than at Penge East and Dulwich (pp. 316/7). Particularly conspicuous is the angled cast ironwork at the corners, which survives at Penge East, Bat & Ball, and Clapham (p.316). Note that the two first-floor lancet windows in the tower were originally glazed, as were some of those at the top of the tower. The full-length roadside canopy lasted only until the early C20, as it had already been cut back to the middle third as now by the 1916 OS. All other details here are unchanged in 2013, save for the disappearance of the additional exit stairs at left.

*The Building News* described the station as 'spacious and convenient.....and of the very best quality.....an unusual amount of decorative taste has been displayed'. It must be remembered that 1862/3 was a relatively early date for such commodious buildings and lengthy canopies at suburban stations, although they were to become seen as the norm for new suburban stations by the 1870s.

The decorative bands, which are the same on the tower as on the main frontage section, are shown correctly (save for one error):

Limestone only, under eaves: Moulded red glazed terracotta plus limestone below, at head of first floor windows: Limestone only, at middle of first floor windows: Moulded red glazed terracotta plus limestone below, at first floor window sill level: Limestone only, under canopy: Moulded red glazed terracotta plus limestone below, at head of ground floor windows: Limestone only, at middle of ground floor windows: Limestone only, at ground floor window sill level (wrongly, it would appear, shown here as moulded red glazed terracotta plus limestone below).





Now that most of the remaining buildings have been cleaned up, it is much more evident why they were seen as striking by contemporaries. This is the front of the eastern pavilion at Penge East, and shows features that were standard at all the stations (with exceptions at West Dulwich, opposite below). London stock brick is used principally, with red brick for the dentillated courses under the eaves (or across the gable end here). There are multiple moulded glazed terracotta bands of reddish hue, each with a flush white limestone course below; plus other bands consisting of a flush white limestone course only. The window and door openings have red brick pointed arches with white limestone impost stones and keystone, each with a trefoil decoration; and an outer arch of blue engineering brick. The circular (œil-de-bœuf) openings are only found in the gable ends at Penge East and Bat & Ball.



The Booking Hall doorways here at Penge East and most of the other stations (cf Herne Hill p.314) have these higher pointed arches, with timber 'plate tracery' above the door.



Detail of the upper part of the tower at Herne Hill (which housed a water tank). If one looks carefully at the brickwork of the second and fourth openings, which were originally glazed (open?), it is seen to be different. There is no trefoil decoration in the impost stones of these arches. A raised limestone bands runs across the foot of the blind openings. Below that is a moulded red terracotta plus limestone band; below which the bands are described at p.313. Between the pointed arches and the red brick dentillation under the eaves, there is a single proud course of ordinary stock brick.

Detail of the frontage at West Dulwich, which is very much the 'odd man out' amongst these stations. It is built principally of this cream brick instead of stock brick. The window and door openings are round-headed, and specially-shaped red bricks are utilised to create the pointed arch above, requiring a large keystone with an extended 'stem and leaf' decoration below the trefoil. The two red moulded glazed terracotta bands are much smaller, having the depth of an ordinary brick course only, and there are no white limestone bands – only this one-course band of red brick halfway between the red moulded courses. Unusually there is also a red brick plinth.







*Left:* The rear of the Clapham building has standard type window and door openings. The dentillation under the eaves is virtually the same as on the Herne Hill tower. Note that here there are some red moulded courses *without* a limestone course below.



*Right:* Silhouette of one of the angled pieces of cast ironwork at the roof corners at Clapham.



At West Dulwich the line is on embankment, with the single-storey building at street level below. The significant differences between the details of this building and the others have been noted already at p.311. The 'Taylor' tiles here lasted into the 1930s, as seen here. The platforms at West Dulwich had timber shelters (now gone), not standard 1862/3 type canopies.

*Courtesy John Minnis*



A more recent view of West Dulwich after cleaning of the brickwork.

*Jim Connor*



Penge retained its 'Taylor' tiles until the early C20. The date of removal of the roadside canopy is unknown, as the OS maps are indeterminate on this point. Hiding as it did the decoration of the middle part of the frontage, it is doubtful if it improved the appearance of the building. The two canopy columns had wooden brackets in the same spirit as the Herne Hill canopy brackets.

*J.E. Connor collection*



The same scene in the early 1960s. The house entrance acquired a small canopy on removal of the main canopy. At left is the goods shed which, like Clapham's, was built in the same style as the station building. It has since been demolished. (The only other station to have a goods shed was Camberwell but no photographs of it are known). Note the chimney style used for all these stations.

*J.E. Connor collection*

Elephant & Castle Station



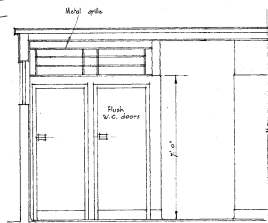
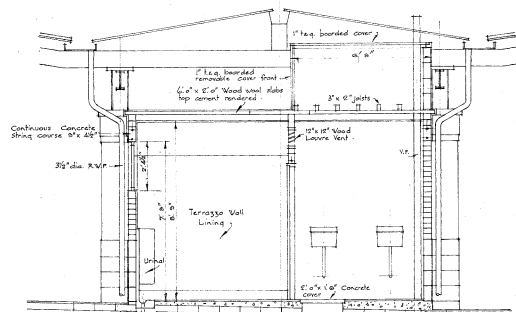


Elephant & Castle Station – Early 20<sup>th</sup> century

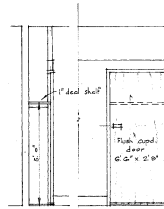




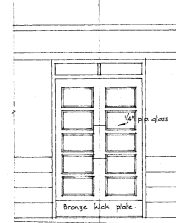




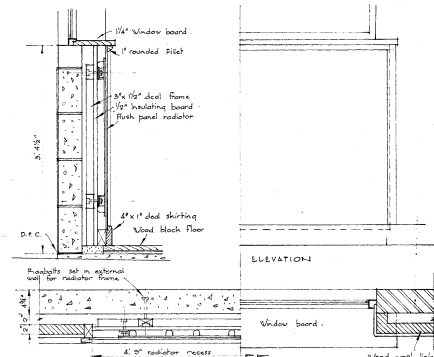
W.C. DOORS - GENTS COURT.



CUPBOARD IN GENTS COURT.

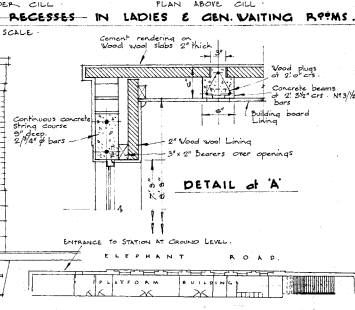
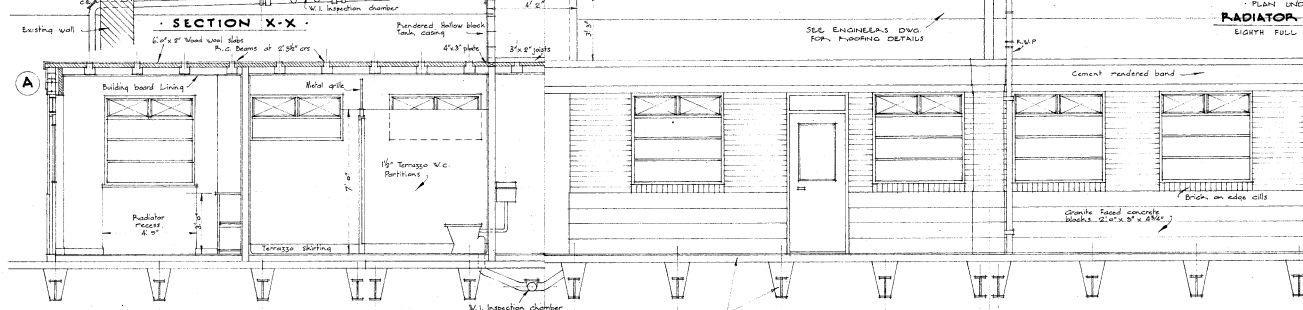


DOORS & GENERAL WAITING ROOM.

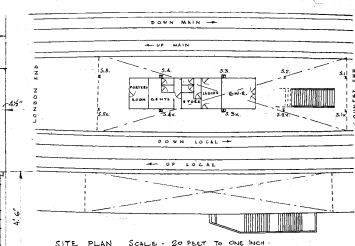
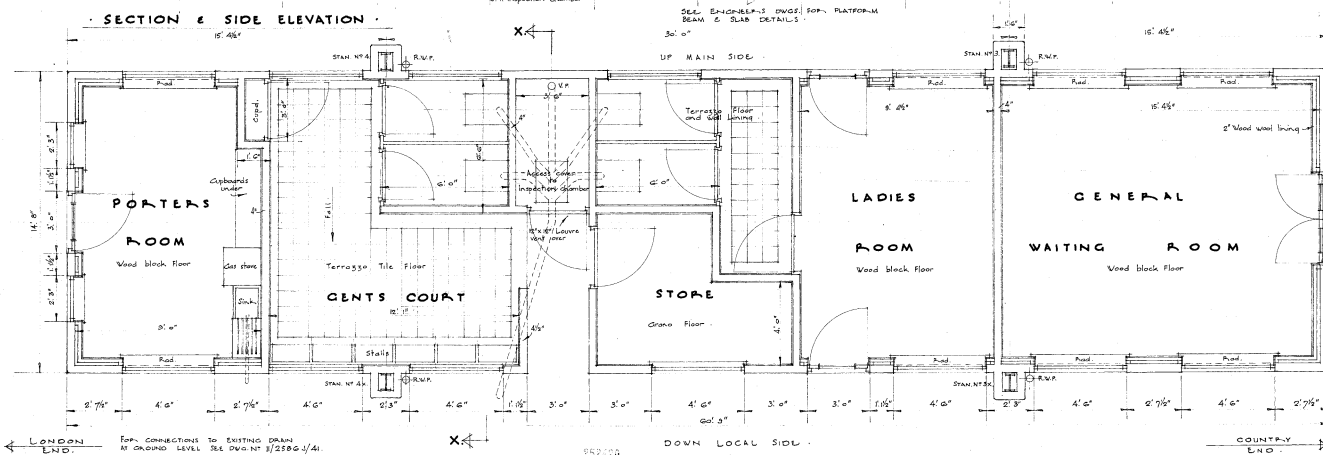


ELEVATION

PLAN UNDER GILL RADIATOR RECESSES - IN LADIES & GEN. WAITING ROOMS. LIGHT FULL SCALE.



DETAIL of 'A'



BRITISH RAILWAYS - SOUTHERN REGION  
ELEPHANT & CASTLE.  
NEW ISLAND PLATFORM BUILDINGS.

SCALE - HALF INCH TO ONE FOOT.

1:3.48 PERMITTED FROM DWG. NO. 3/2586/30

NEG. NO. 47077.

Elephant & Castle Station – Post war



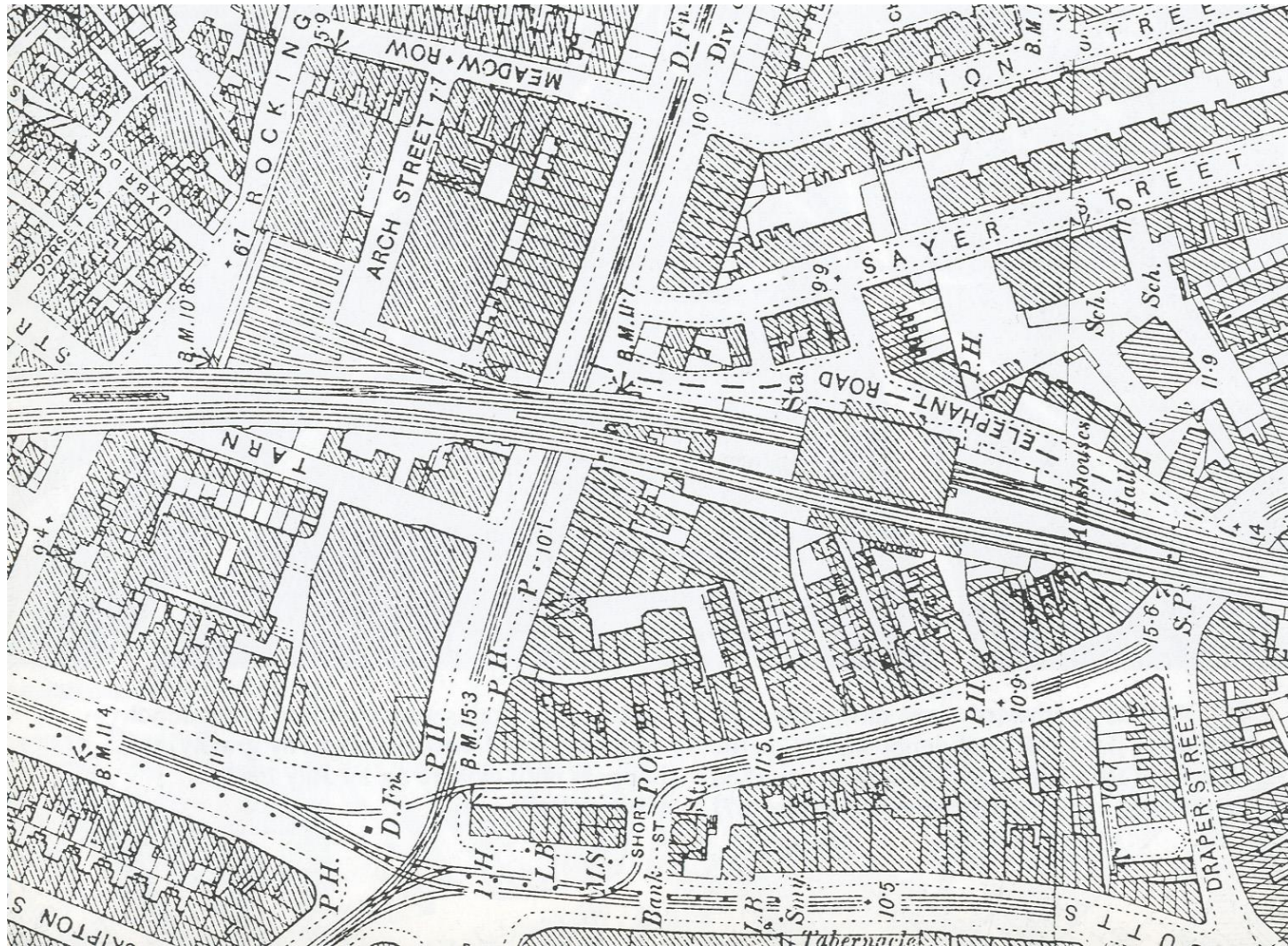


## Elephant & Castle Station – Post War





# Elephant & Castle Station – 1895





Elephant & Castle Station  
Proposed Front  
(East) Elevation  
1:150 @ A3



Elephant & Castle Station

DESIGN STUDY

Copyright Benedict O'Looney Architects 2021. Do not scale from these drawings.  
Verify all dimensions on site and report any discrepancies to the architect / contract administrator immediately.  
All structural elements to be designed by the structural engineer.

SCALE: 1:150 @ A3  
1:75 @ A1

DRAWING: EAC\_p\_150\_e1  
DATE: 27/03/23

Benedict O'Looney Architects  
56 Pennethorne Road Peckham  
SE15 5TQ t: 0207 732 9713

# General notes

The 1920's pre-cast ferro-concrete platform parapet walls are to be clean and checked for defects. Keim lime wash to be applied to create a uniform finish.

The large concrete brackets to the 1920's platform extensions to be are to be cleaned and checked for defects. Specialist concrete repair contractors will be required to repair lost areas of concrete cover. Keim lime wash to be applied to create a uniform finish. Colour to be confirmed.

## Elephant & Castle Station Proposed Front (East) Elevation Southern portion

The dashed line indicates the original Victorian roof and lantern that was removed following wartime damage.

Rainwater goods to be inspected, checked for defects and painted-in to harmonise with the brick / masonry detailing.

The platform level windows are to be 'opened up' by removing the current undersized windows and masonry infill. These openings are to be filled with hardwood timber sash window as shown in the archive drawing. These are to be painted with 'Weathershield' type gloss, RAL 6011 t.b.c. (1.1)

new lift position as per the option 3 scheme.

The paired widows to the south side of the elevation to be reinstated to their original size.

The 1920's crittal windows to be cleaned and refurbished where necessary and re-painted to the new window paint scheme. Colour tbc.

A new ramp and bronze stainless steel handrail provides access for all to the station ground level from Elephant Road. The handrail detailing is inspired by the inter-war metalwork detailing found elsewhere in the station, and on many other Southern Railway stations improved in the mid 20<sup>th</sup> century period (1.3)

0 1m 2m 3m 4m 5m

Elephant & Castle Station

Copyright Benedict O'Looney Architects 2025. Do not scale from these drawings. Verify all dimensions on site and report any discrepancies to the architect / contract administrator immediately. All structural elements to be designed by the structural engineer.

SCALE: 1:75 @ A3

DRAWING: EAC\_p\_75\_e1

DATE: 27/03/23

Benedict O'Looney Architects  
56 Pennethorne Road Peckham  
SE15 5TQ t: 0207 732 9713



# General notes

The 1920's pre-cast ferro-concrete platform parapet walls are to be clean and checked for defects. Keim lime wash to be applied to create a uniform finish.

The large concrete brackets to the 1920's platform extensions to be are to be cleaned and checked for defects. Specialist concrete repair contractors will be required to repair lost areas of concrete cover. Keim lime wash to be applied to create a uniform finish. Colour to be confirmed.

## Elephant & Castle Station Proposed Front (East) Elevation Northern portion

The dashed line indicates the original Victorian roof and lantern that was removed following wartime damage.

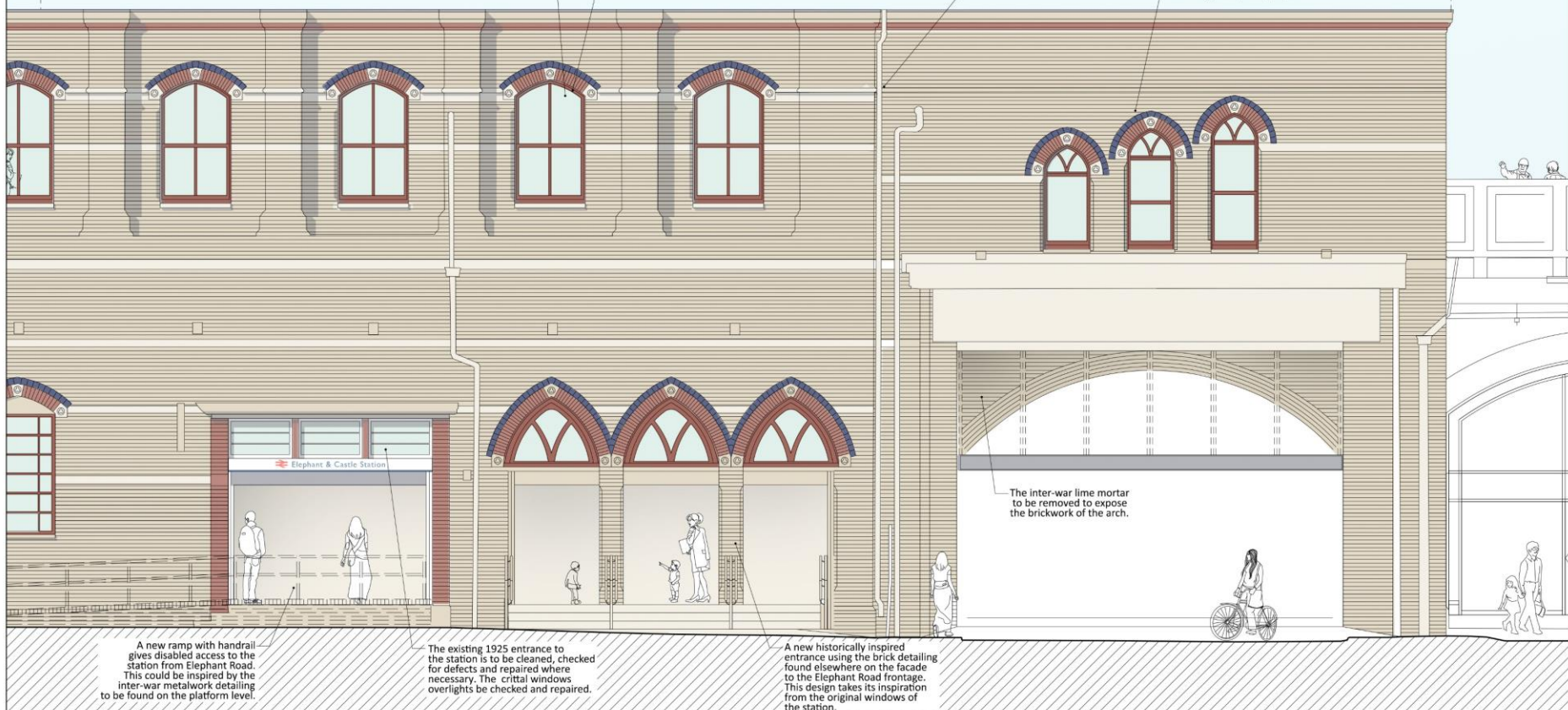
new lift position as per the option 3 scheme.

The platform level windows to be 'opened up' by removing the current undersized windows and infill to be replaced by a timber sash window as shown in the archive drawings.

New sash windows to be painted. Suggested colour oxide red to compliment the brick work.

Rainwater goods to be inspected, checked for defects and painted-in to harmonise with the brick / masonry detailing.

The triplet window to the north part of the facade to be carefully re-opened and new period-style hardwood widows are to be introduced taking their cue from archive drawings and photographs.



A new ramp with handrail gives disabled access to the station from Elephant Road. This could be inspired by the inter-war metalwork detailing to be found on the platform level.

The existing 1925 entrance to the station is to be cleaned, checked for defects and repaired where necessary. The critical windows overlights be checked and repaired.

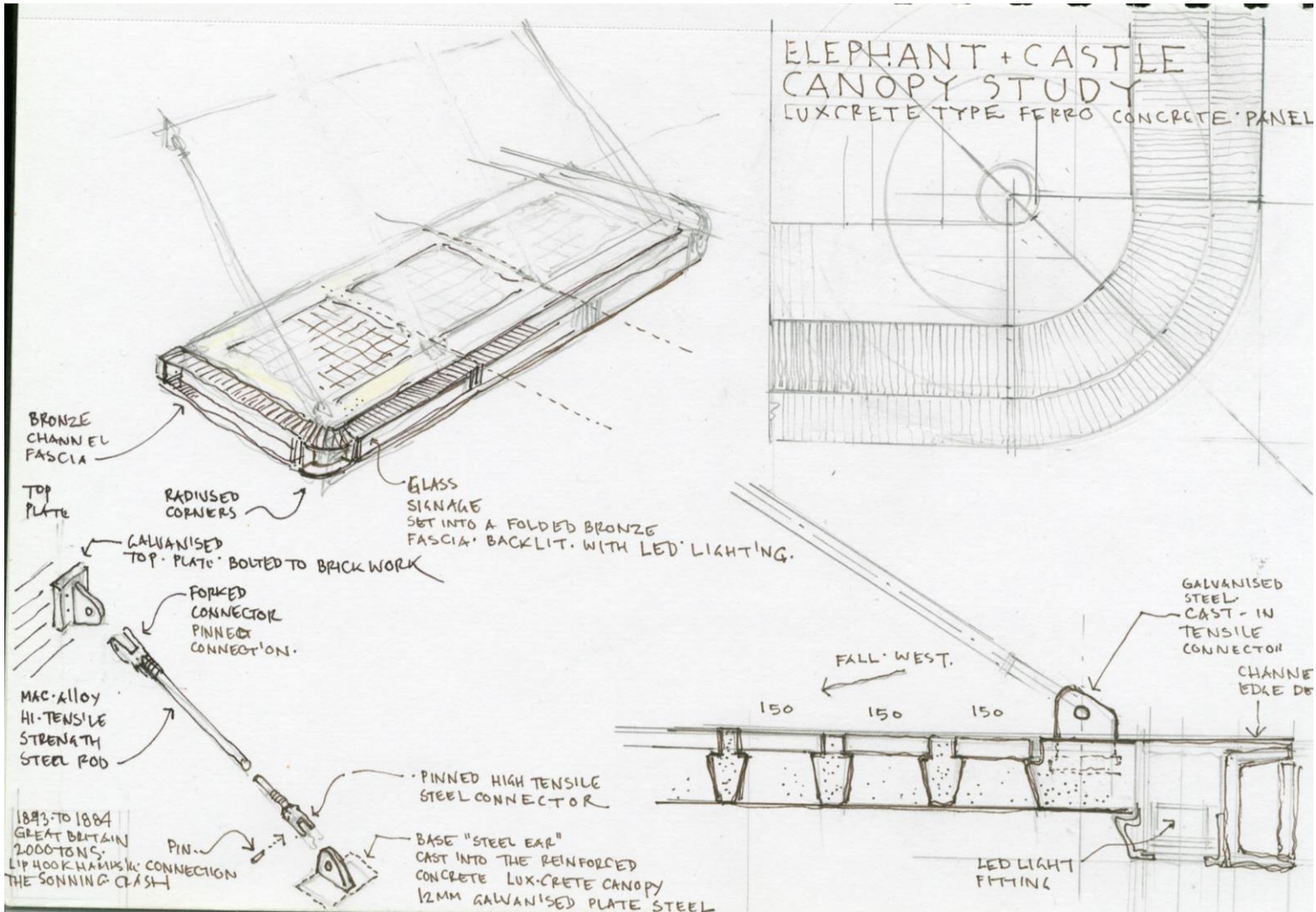
A new historically inspired entrance using the brick detailing found elsewhere on the facade to the Elephant Road frontage. This design takes its inspiration from the original windows of the station.

## Elephant & Castle Station – Southern Railway Architects

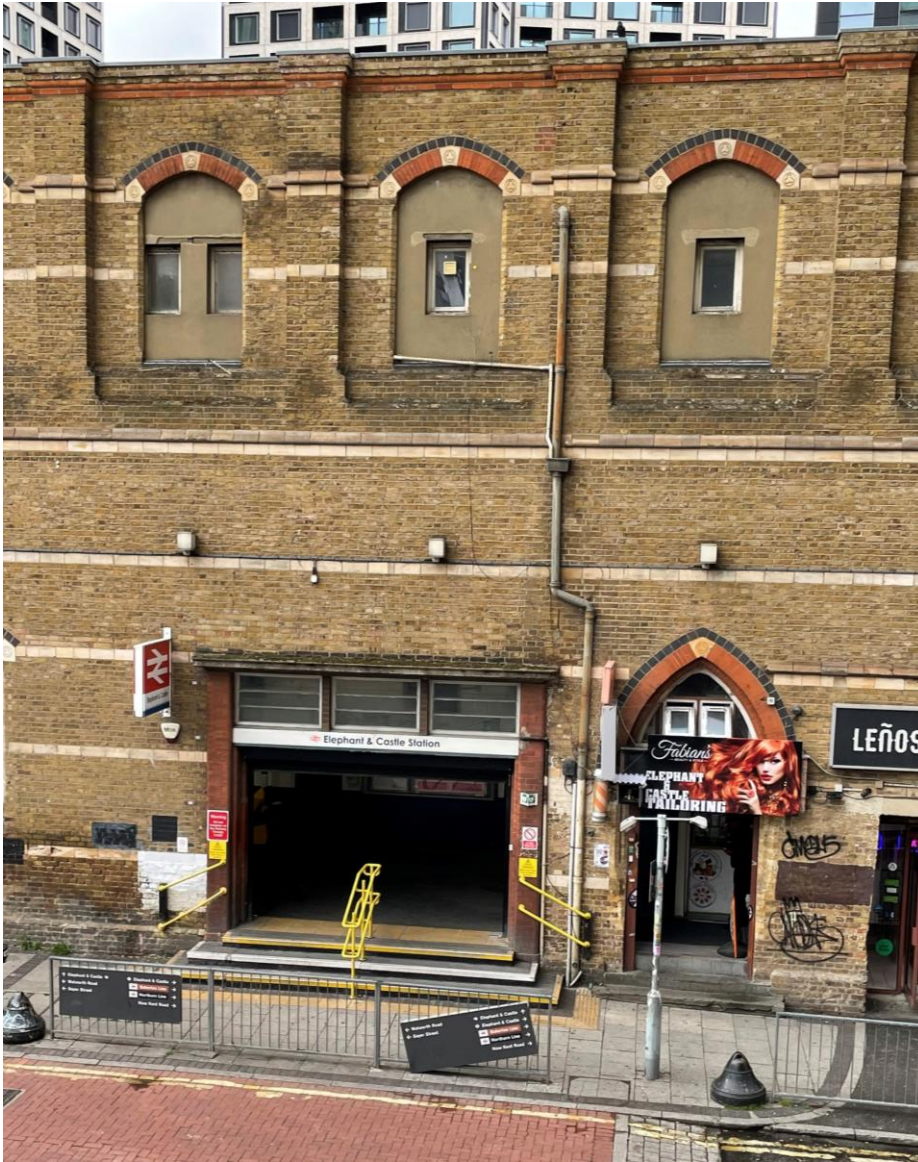




# Elephant & Castle Station – Southern Railway Architects

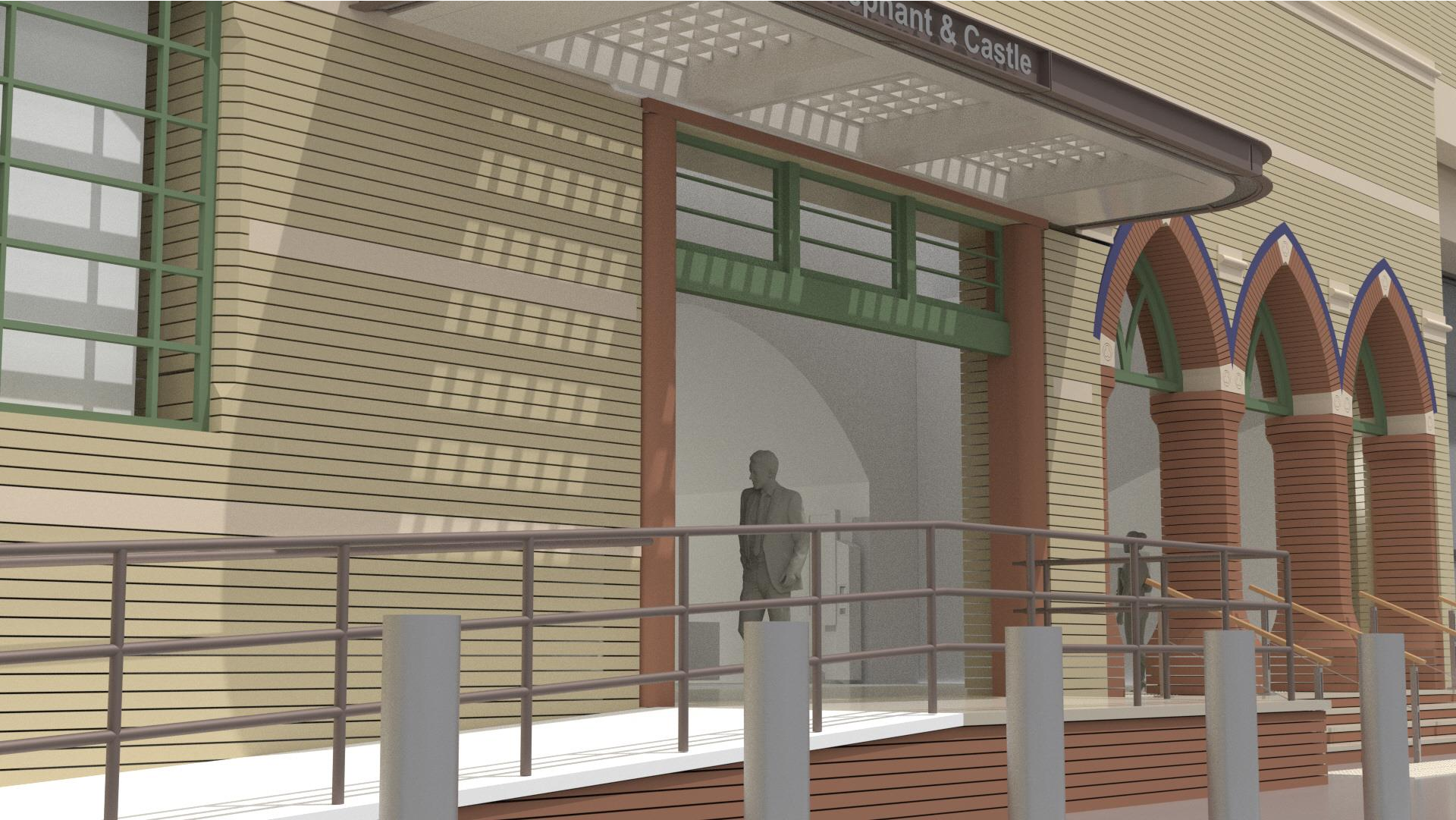


## Elephant & Castle Station – LC&D rwy. & Southern Railway Architects, 1928 / 1950



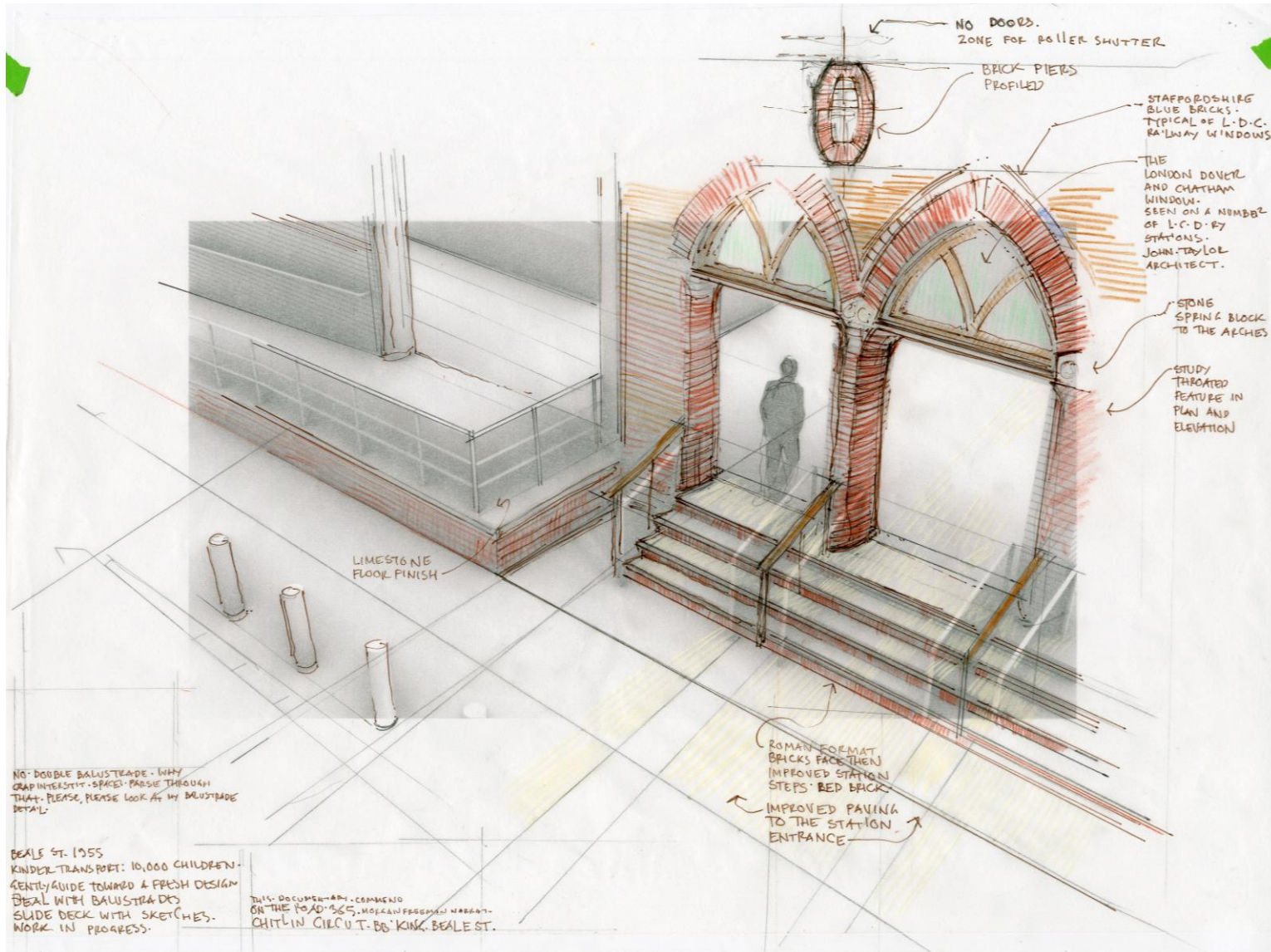


## Elephant & Castle Station





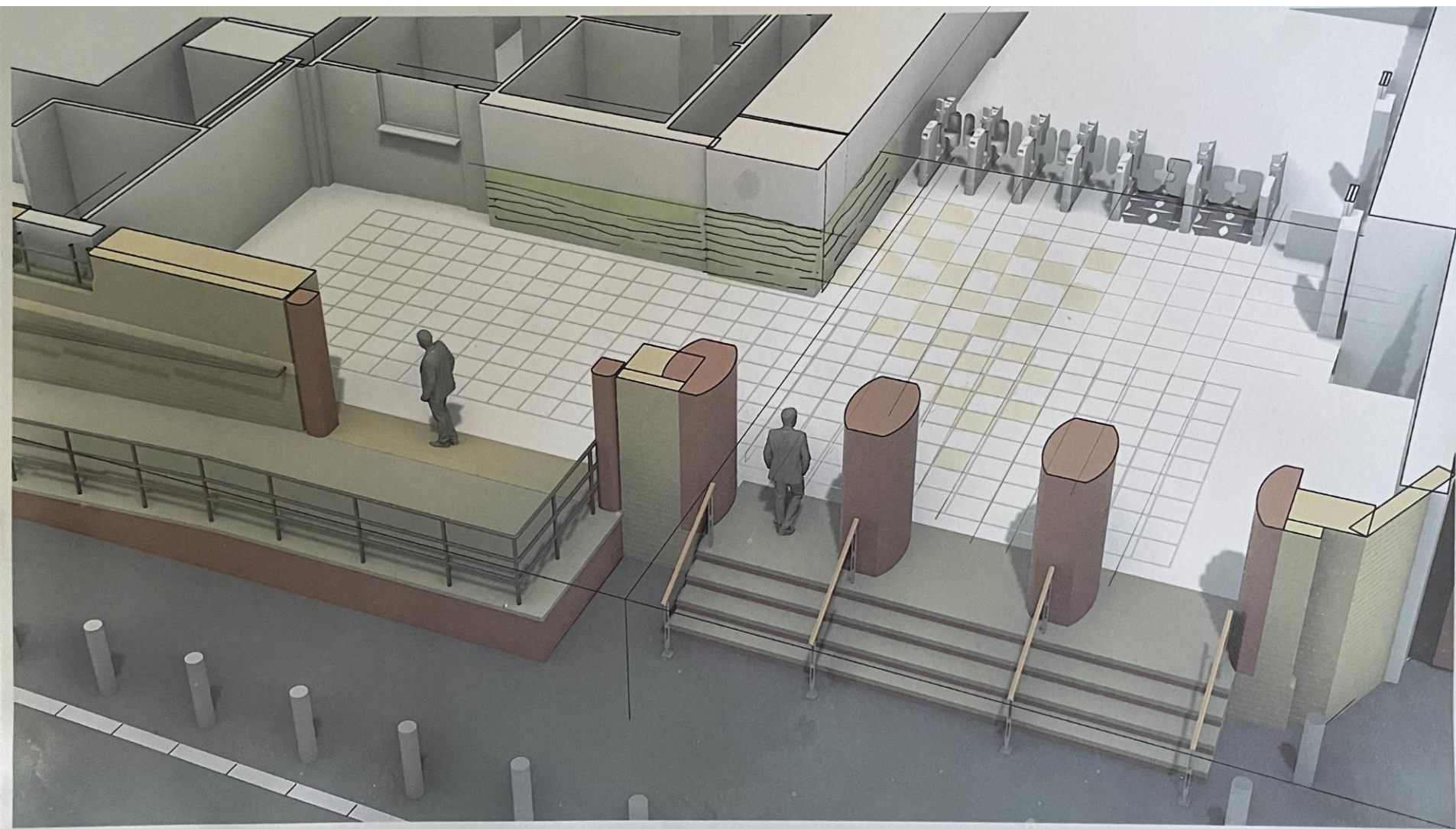
# Elephant & Castle Station – Elephant Road Designs





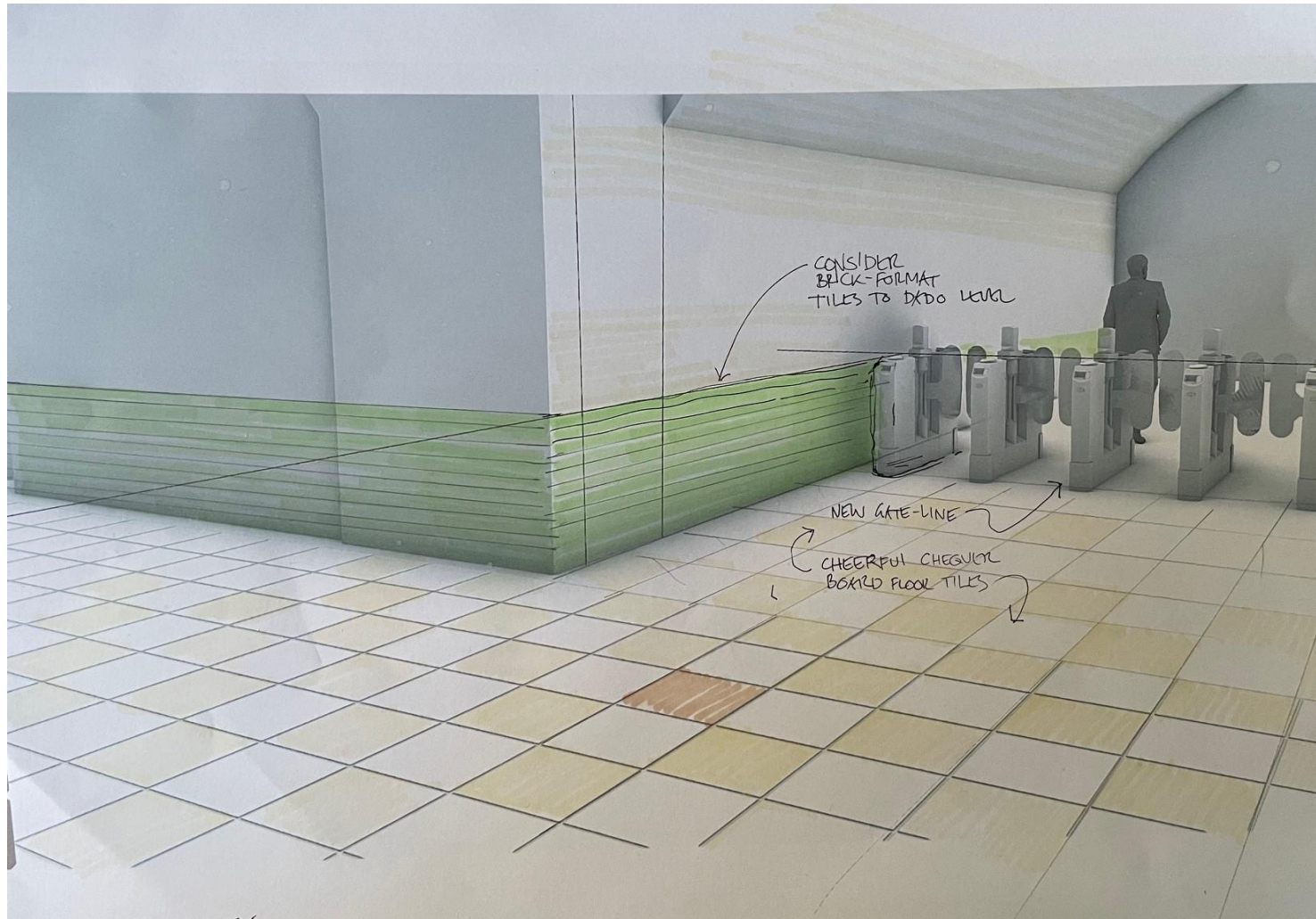
# Elephant & Castle Station – Southern Railway Architects







## Elephant & Castle Station – design studies



## Main Historical Façade Entrance

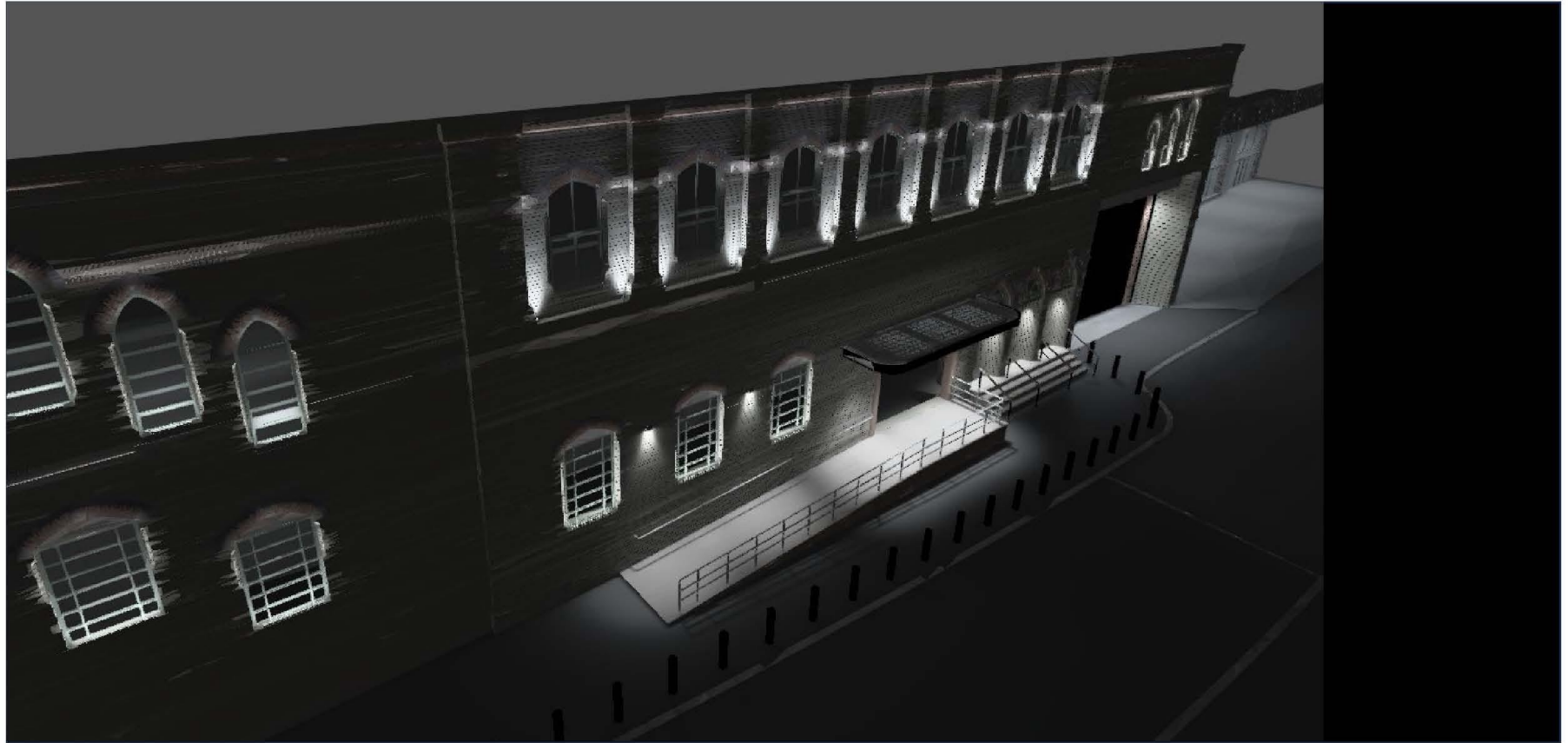
### 3D model Overview





## Main Historical Façade Entrance

### 3D model Overview



## Main Historical Façade Entrance

### Overview



1

Window reveals illuminated with Linealuce 27 wall grazing wide flood optics mounted onto the windowsill. Either 329mm, 625mm or 920mm in length, depending on the size of the window bay.

Art: UE28 / UE56 / UE84

2

Upper window bays are flanked with columns. These are possibly the strongest architectural elements on the façade and have therefore been highlighted. Two iPro uplighters have been positioned to light both the column sides and the main brick façade. The cornice above will also receive light.

Art: BK25







## Main Historical Façade Entrance

### 3D model Overview





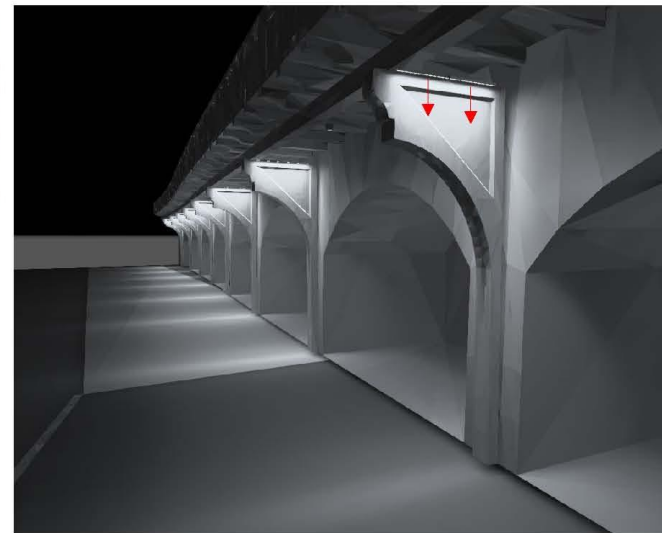
## Pillars & Walkway

### Overview

**1**

The upper portion of the concrete pillars are illuminated using 2Nr Linealuce 27 wall wash optic, linear down lights. These provide a strong vertical lighting element which is repeated the length of the station.

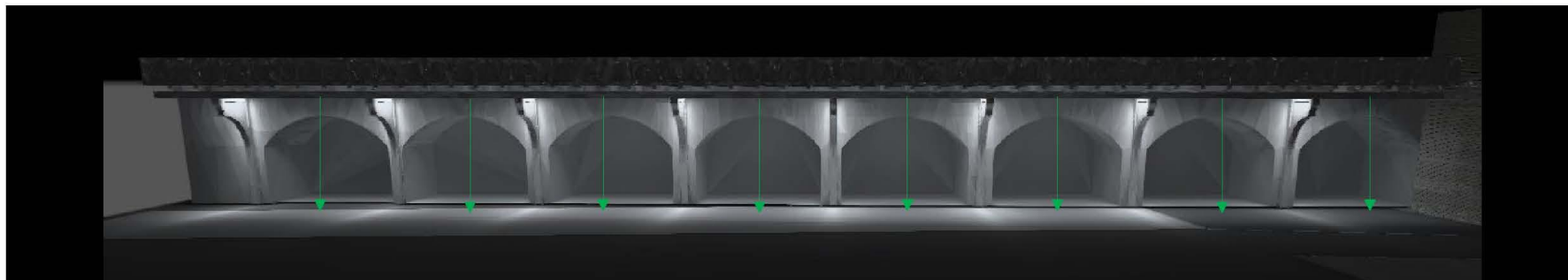
Art: UE84

**2**

A 155mm iPro flood light has been proposed, centrally above each arch. These provide the functional lighting over the pavement below.

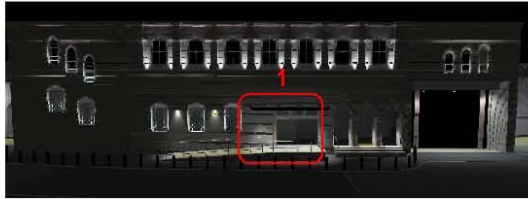
Light Level (pavement) = 50Lux 0.40Uo

Art: EP65 + BZ67 + BZ63



## Main Historical Façade Entrance 1

### Overview



1

Entrance 1 - illumination covering the top of the ramp.  
Using Laser Blade 5 cell down lights mounted on the  
underside of the canopy within the 'skylight element'.

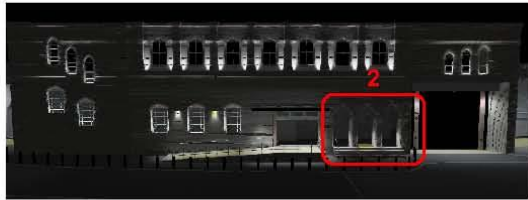
Art: E886





## Main Historical Façade Entrance 2

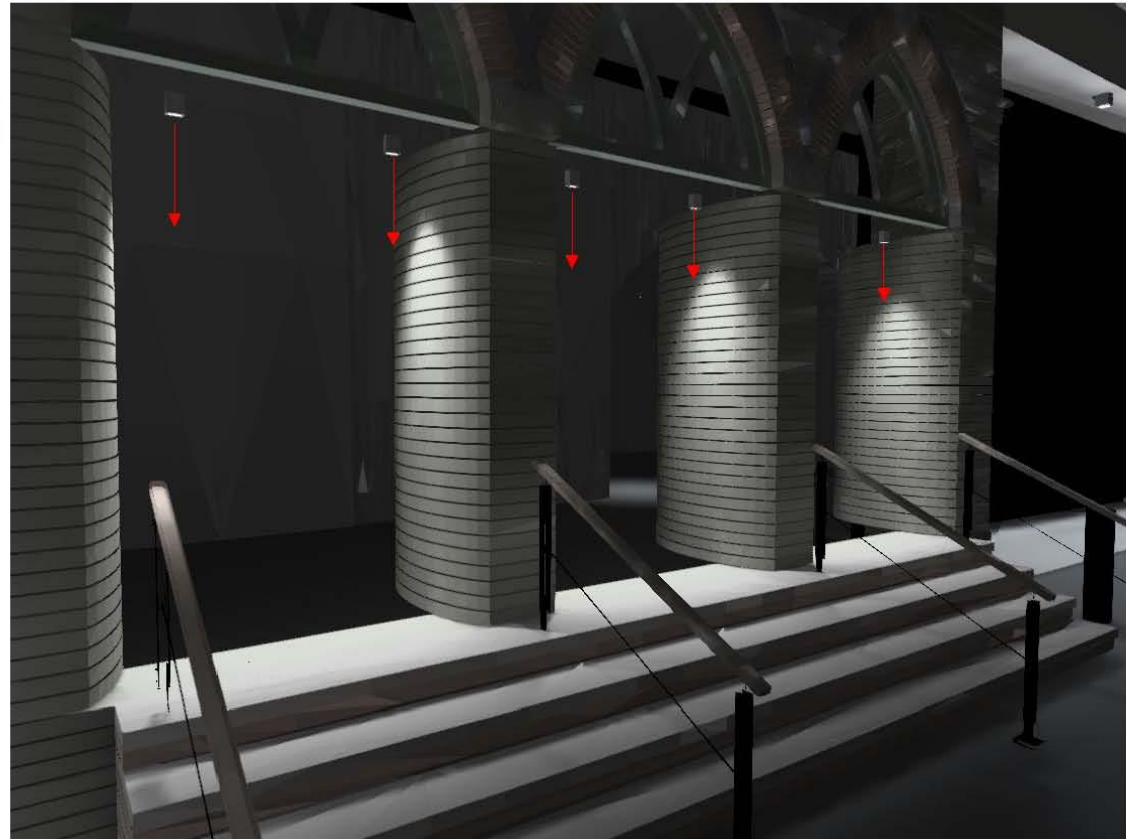
### Overview



2

Entrance 2 - lighting over the entrance threshold and steps. Laser Blade in/out 2 cell down lights. 50-150Lux. Vertical lighting provided over brick columns to hi-light the entrance.

Art: E882



## Main Historical Façade Entrance 3

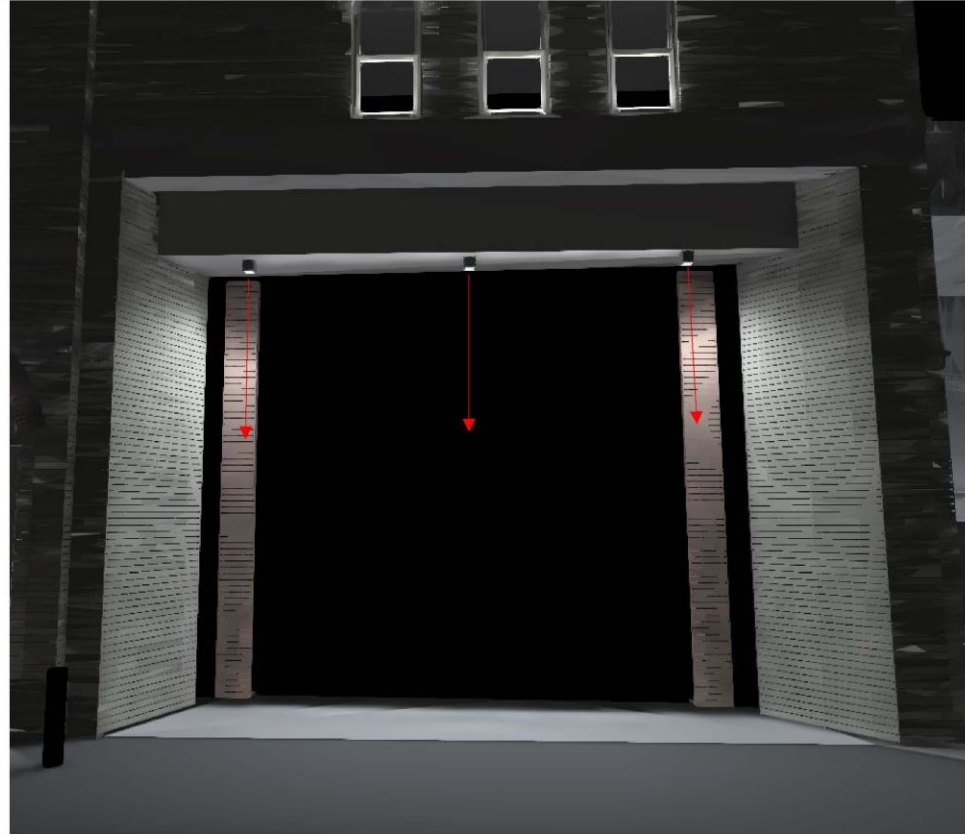
### Overview



3

Entrance 3 - illumination over the threshold using three iPro surface mounted down lights. Again, attention has been made to make sure the vertical surfaces around the entrance are well illuminated. 100lux is provided over the pavement threshold area.

Art: EP65 + BZ67 + BZ63



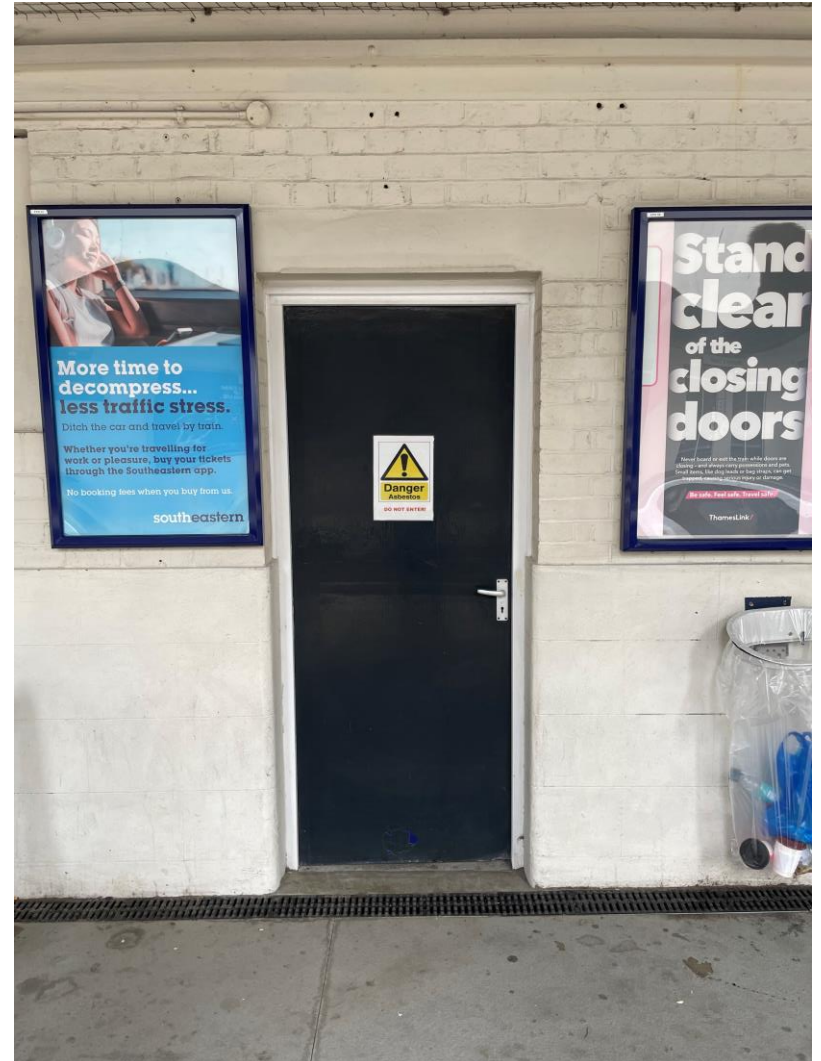


Elephant & Castle Station – Early 20<sup>th</sup> century



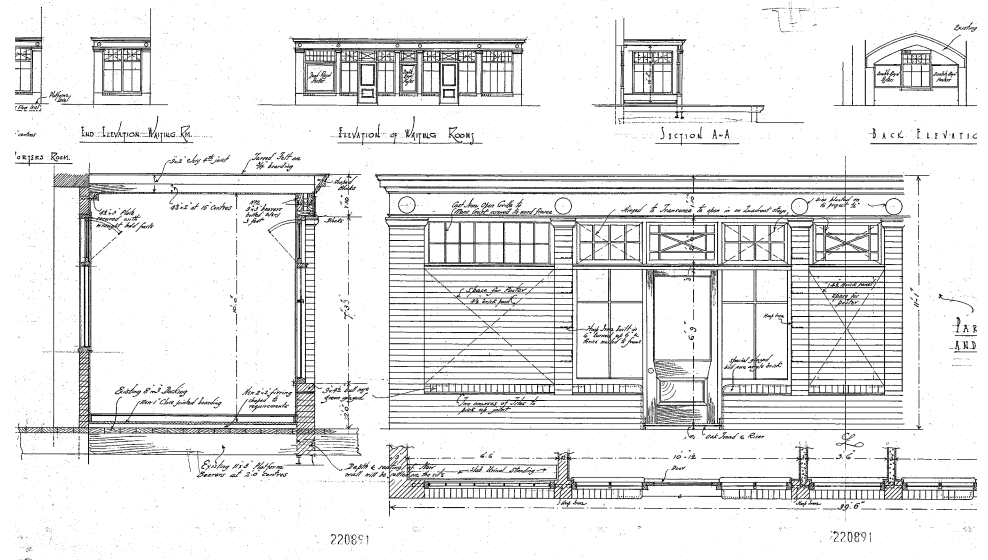


## Elephant & Castle Station – Southern Railway Architects





# Elephant & Castle Station – Southern Railway Architects







21 min  
21:20 Sevenoaks  
Thameslink service formed of 8 coaches. 1  
3rd 22:20 Sevenoaks  
On time  
20:58:47



This Public Facility will  
remain open  
between the hours  
0600 to 0600 - Monday  
to Sunday  
0600 to 0600 - Sunday

0 Day Charge  
Thameslink  
Thameslink  
Thameslink





## Elephant & Castle Station – Southern Railway Architects



# Elephant & Castle Station – Southern Railway Architects circa 1930 and 1950

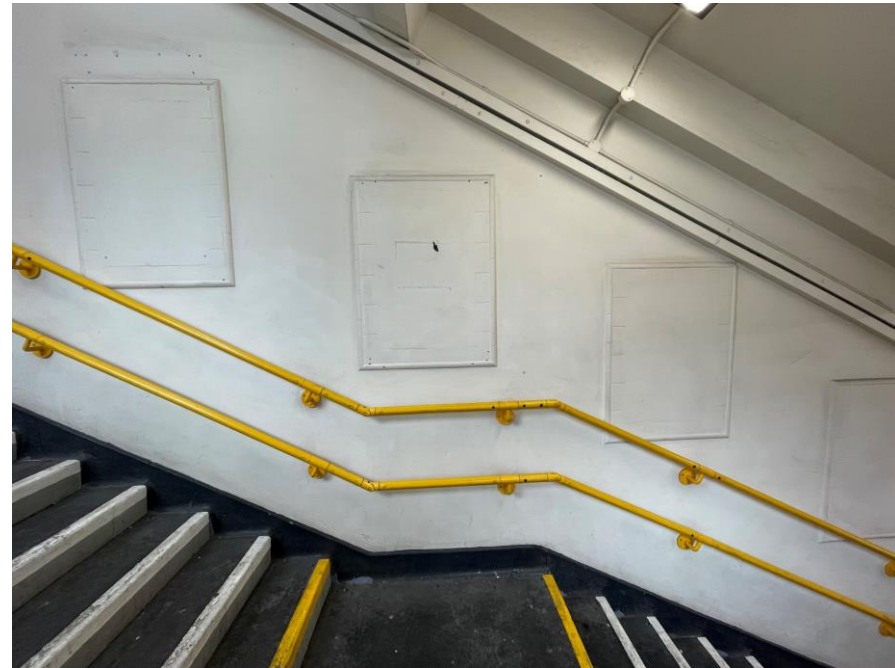




# Elephant & Castle Station – Southern Railway Architects circa 1930 and 1950



# Elephant & Castle Station – Southern Railway Architects circa 1930 and 1950





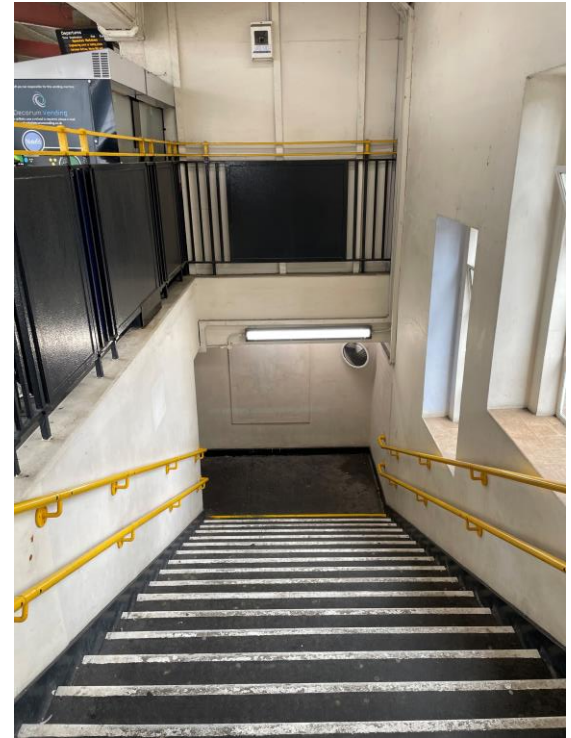
Richmond Station – Southern Railway Architects 1935 – Stair detailing



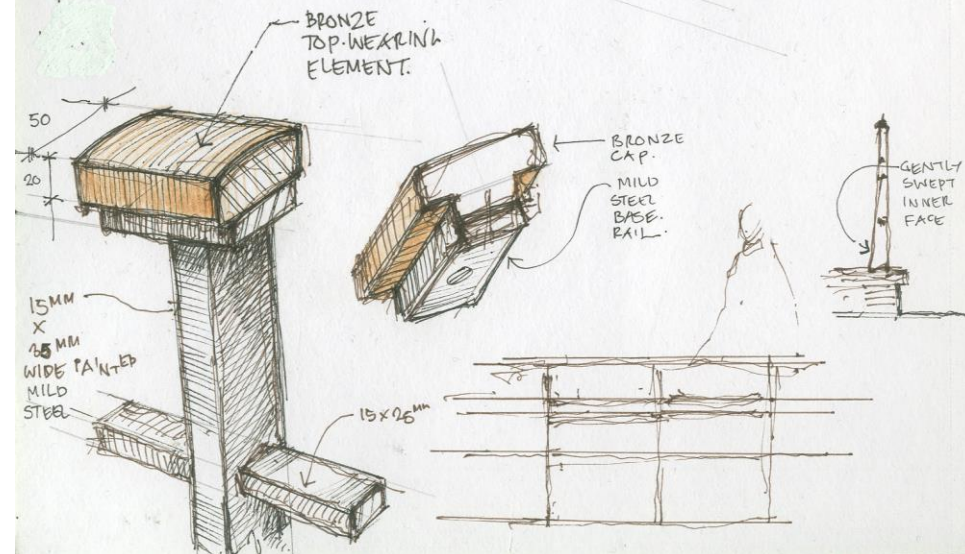
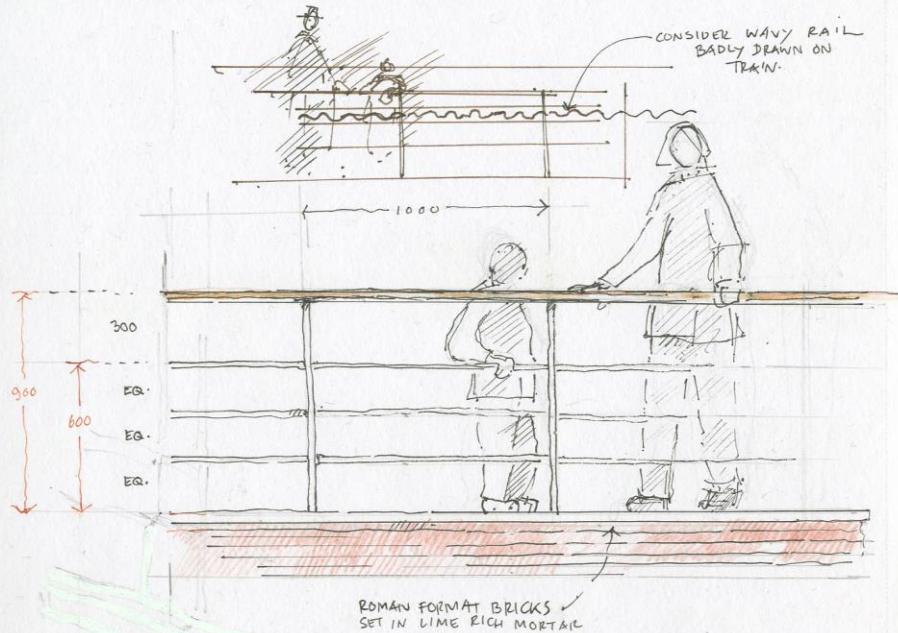




# Elephant & Castle Station – Southern Railway Architects circa 1930 and 1950



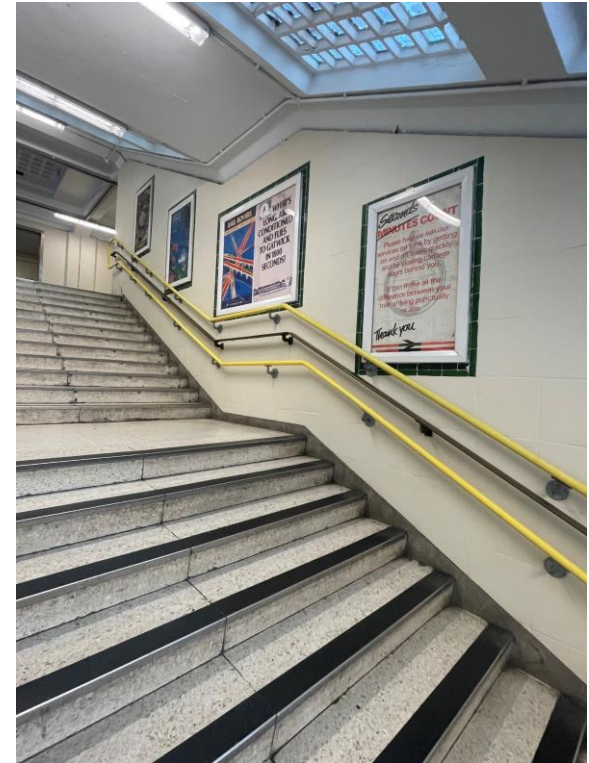
# HANDRAIL DESIGN INSPIRED BY THE SOUTHERN RAILWAY



## Elephant & Castle Station – detail studies



## Richmond Station – Southern Railway Architects 1935 – Restored front bronze canopy



Elephant & Castle Station  
Proposed Front  
(East) Elevation  
1:150 @ A3



Elephant & Castle Station

DESIGN STUDY

Copyright Benedict O'Looney Architects 2021. Do not scale from these drawings.  
Verify all dimensions on site and report any discrepancies to the architect / contract administrator immediately.  
All structural elements to be designed by the structural engineer.

SCALE: 1:150 @ A3  
1:75 @ A1

DRAWING: EAC\_p\_150\_e1  
DATE: 27/03/23

Benedict O'Looney Architects  
56 Pennethorne Road Peckham  
SE15 5TQ t: 0207 732 9713









































INDIA

itc





FINDLATER'S



BOROUGH HIGH STREET



EXPRESS  
DAIRY  
C<sup>o</sup> LTD

EXPRESS  
DAIRY  
C<sup>o</sup> LTD

AFTERNOON  
TEAS  
LUNCHEONS  
SMOKING ROOM

AFTERNOON  
TEAS  
LUNCHEONS  
LADIES ROOM





A mosaic sign with a blue background and a green border. The text is in large, yellow, outlined letters. The sign reads: AFTERNOON TEAS LUNCHEONS LADIES ROOM.

AFTERNOON  
TEAS  
LUNCHEONS  
LADIES ROOM