

Elephant & Castle Station – Heritage Opportunities

November 19 2025

Benedict O'Looney Architects

Richmond Station – Southern Railway Architects 1935 – Restored front elevation



RICHMOND STATION

RICHMOND



Richmond Station – Southern Railway Architects 1935 – Restored front bronze canopy



Richmond Station – Southern Railway Architects 1935 – Stair detailing





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Richmond Station – Southern Railway Architects 1935



Richmond Station – Southern Railway Architects 1935



Richmond Station – Southern Railway Architects 1935



Richmond Station – Southern Railway Architects 1935 – Stair detailing

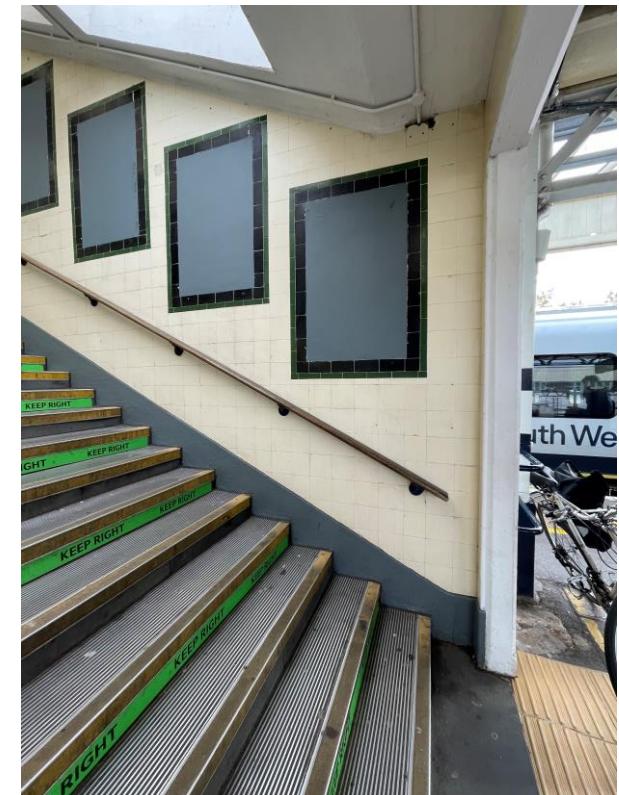
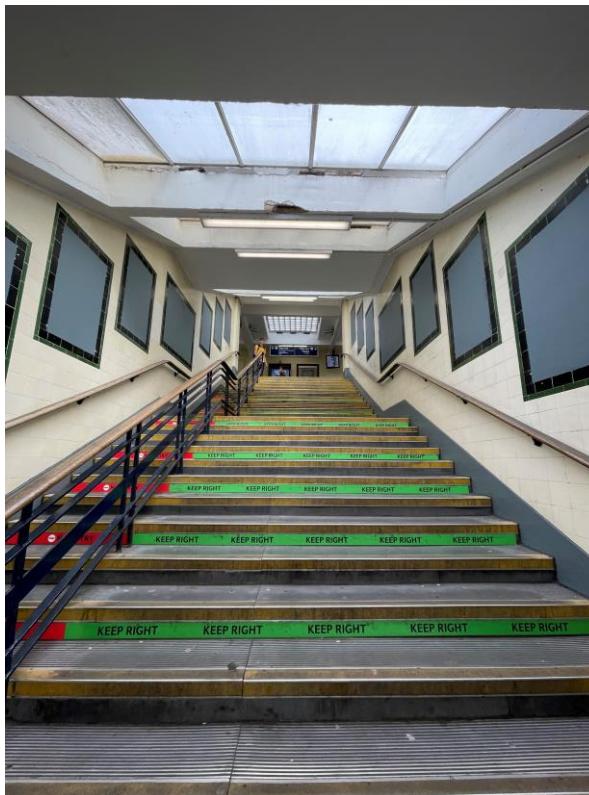


Richmond Station – Southern Railway Architects 1935 – passenger bridge



Surbiton Station – Southern Railway Architects 1935

Stairs to the platforms



Richmond Station – Southern Railway Architects 1935 – Using period posters in the restored tiled poster surrounds



Richmond Station – Southern Railway Architects 1935 – Using period posters in the restored tiled poster surrounds



Richmond Station Booking Hall – Southern Railway Architects, 1935 – Using period posters with black tile and travertine surrounds – Summer 2025



Richmond Station – restoration



RICHMOND RAILWAY STATION - A HISTORY

Richmond is a significant Art Deco designed station by the Southern Railway Architect's Department from the interwar period (1935), led by their chief architect James Robb Scott (1882-1965).

The Southern Railway was created in 1923 and comprised the networks of the London, Brighton & South Coast Railway, the South Eastern & Chatham Railway and the Great Southern & Western Railway, covering much of southern England. Two generations after these railways' original construction in the nineteenth century, many stations were in need of an upgrade. In 1933, the Southern Railway appointed James Robb Scott as architect for this task that included the modern architect Maxwell Fry.

Other Art Deco stations included Wimbledon, the Grade II listed Surbiton Station, and the striking group of suburban stations on the line to Cheshunt. The original station at Richmond was one of the most prominent in the group on account of the use of polished black granite cladding to the station building and marble wall linings to the interior. But despite this, it has not yet been restored.

By the turn of the 21st Century, Richmond had lost much of its 1930s splendour and character. However, South Western Railway and Network Rail were determined to restore the station to its former glory and began working with the Railways Heritage Trust (RHT) and London Borough Architects. Historic photographs and original contract drawings were consulted to create a detailed restoration plan. The project was funded by Network Rail, the Railway Heritage Trust, the London Borough of Richmond upon Thames and South Western Railway.

This included the discovery and restoration of the green glass freeze above the platform canopy, the original station name signs, and the original signs identifying the different station services. The freeze largely survived under more recent cladding, as well as a veranda cladding to the booking hall walls which had been removed in the 1960s. The original canopy structure also involved re-fabricating the lost bronze cladding to the original 1935 concrete entrance canopy, replacing and restoring to period correct the flagpoles and the original station name signs. New station signs in the same Southern Railway style have been installed. Finally, new shopfronts in the same style have replicated the unoccupied contemporary structures which had appeared in the booking hall in a replicated way over the last thirty years.

To complement the restoration, it was fitting that an original station doorway was reinstated in the booking hall, and a new entrance added to the wall on the other side of the booking hall. The sign is on loan from Transport for London (TfL) after it turned up at a sale and was purchased to be displayed at the station. The original sign is now in the TfL collection. As can be seen in the historic photograph of it (fig. XX), the sign is double-sided; the other side still exists at Mangapps Farm Railway Museum in Burnham-on-Crouch, Essex, and is currently restored.

The restored station fagade and interior was unveiled by Lord Hendy of Richmond Hill on 12 September 2025.

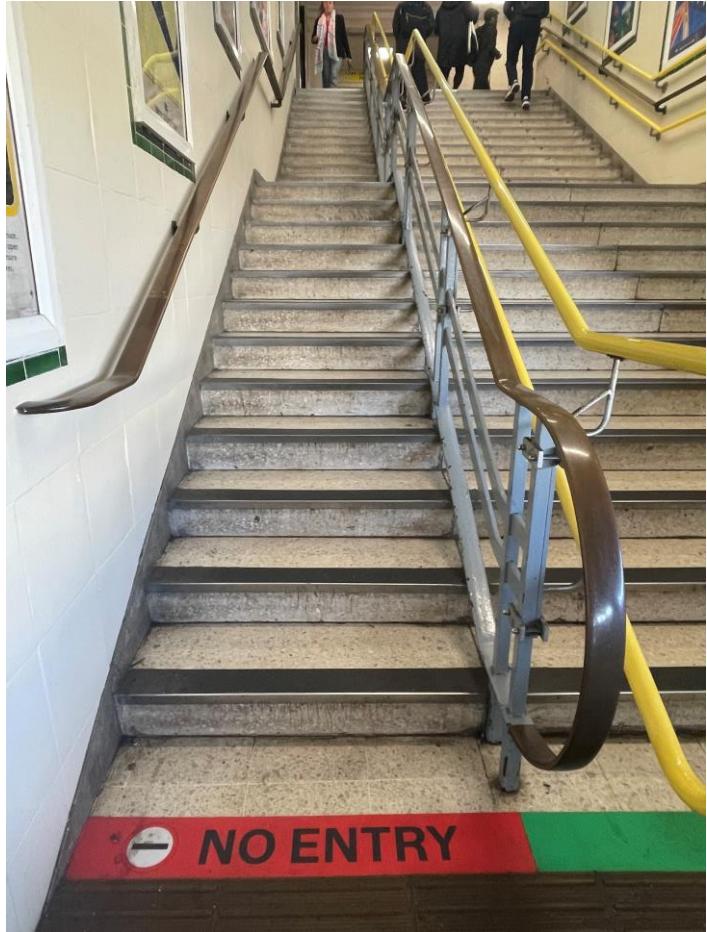
A view of the canopy entrance of the Richmond Station booking hall. A mural depicting the station interior at Richmond's restored Castle Hotel near Richmond Bridge. This photograph shows the original tiled floor which echoes the ceiling design and building's structural grid. The floor tiles survive in the north wing of the station, now a gym.

Surbiton Station (Grade II listed), also designed by James Robb Scott.

South Western Railway Network Rail RAILWAY HERITAGE TRUST LONDON BOROUGH OF RICHMOND UPON THAMES TRANSPORT FOR LONDON

Richmond Station – restoration

Richmond Station – Southern Railway Architects 1935 – Stair detailing



SURBITON STATION

ON CARS

STATION CARS

Station entrance

Station entrance

Station entrance

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SURBITON

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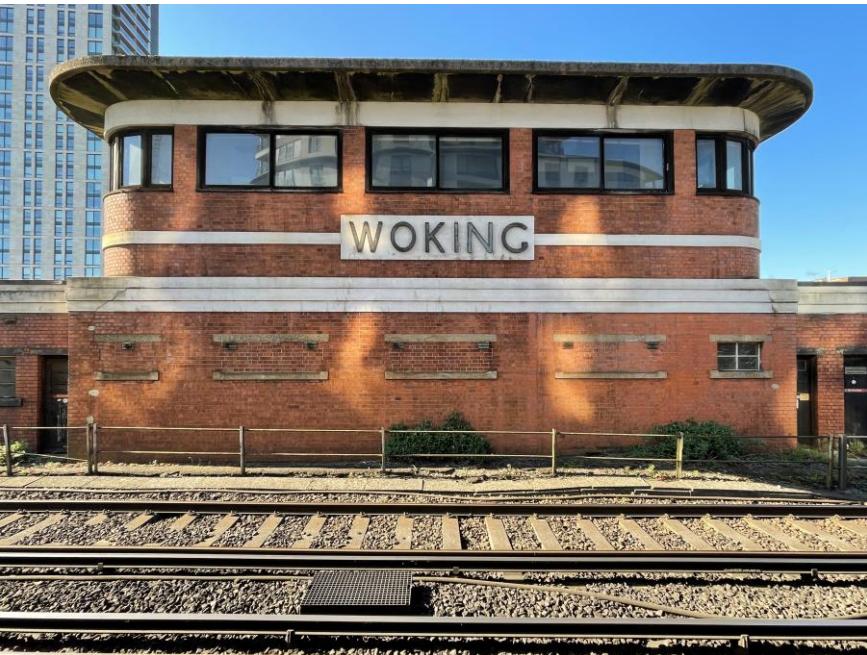


Southern Railway Architects

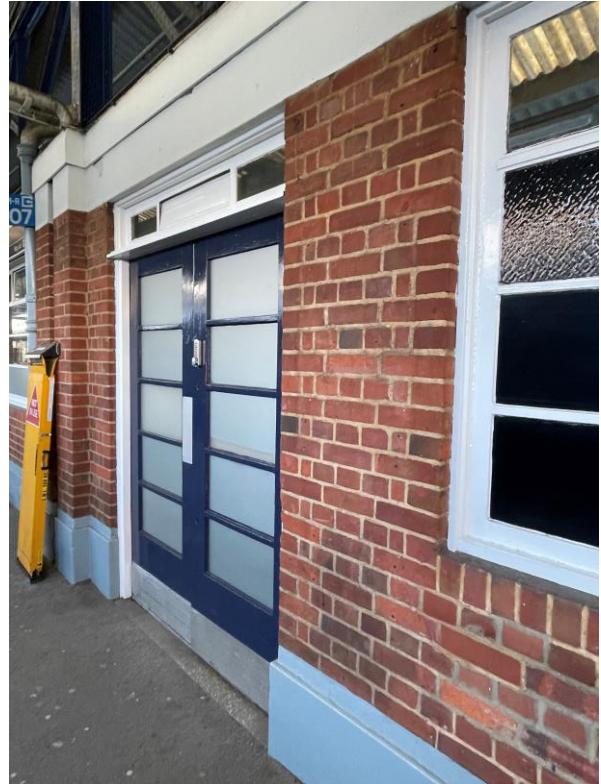




Woking Station – Southern Railway Architects



Woking Station – Southern Railway Architects

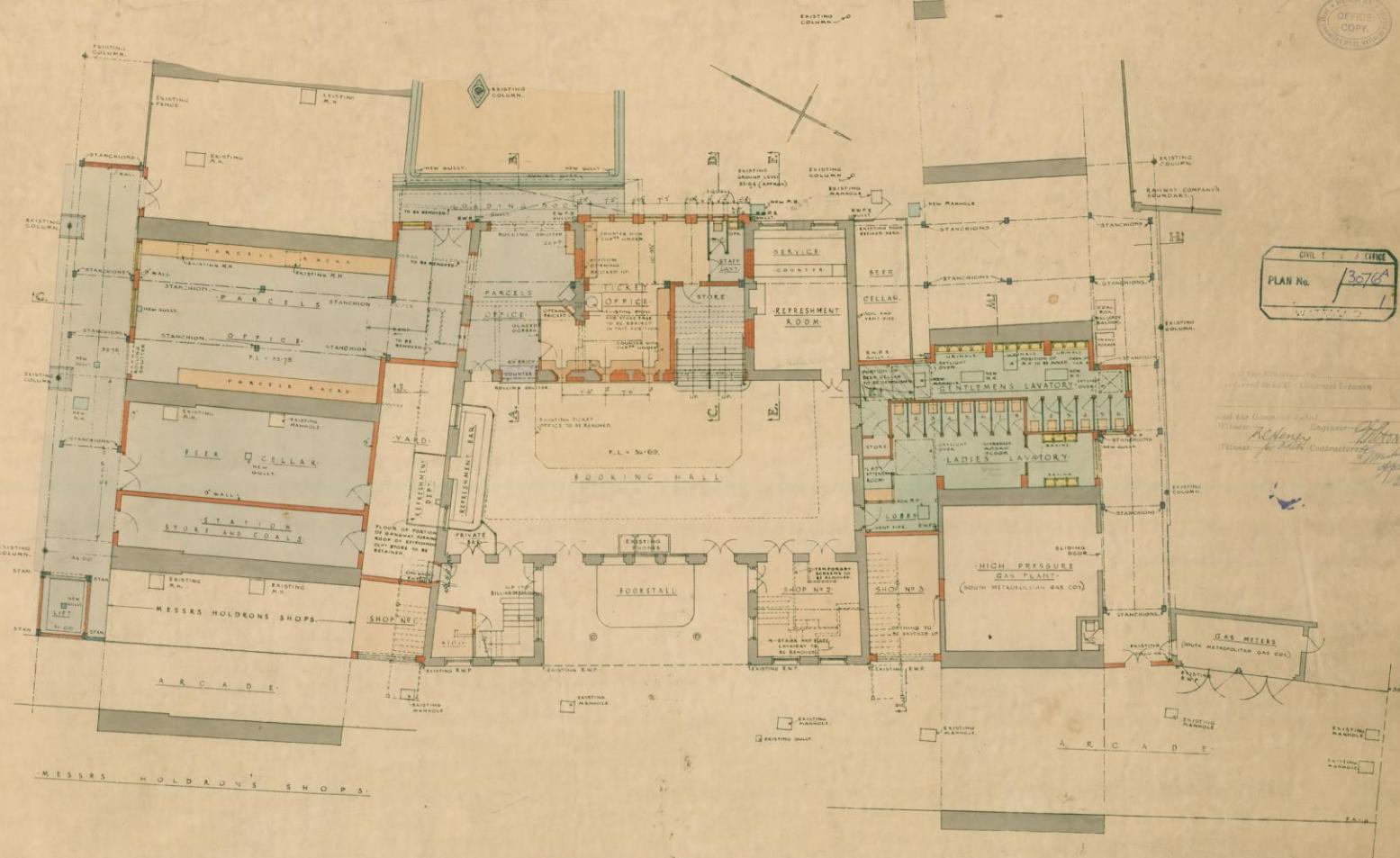




92717728

S.R. PECKHAM RYE STATION IMPROVEMENTS

NOTE FOR DETAILS OF STEELWORK
SEE ENGINEER'S DRAWINGS.



GROUND FLOOR PLAN.

FEET 0 10 20 30 40 50
SCALE: $\frac{1}{8}$ INCH = 1 FOOT

MESSRS. HOLDRODS SHOPS
Pavement Level at
Arcade Entrance = 31'-0"

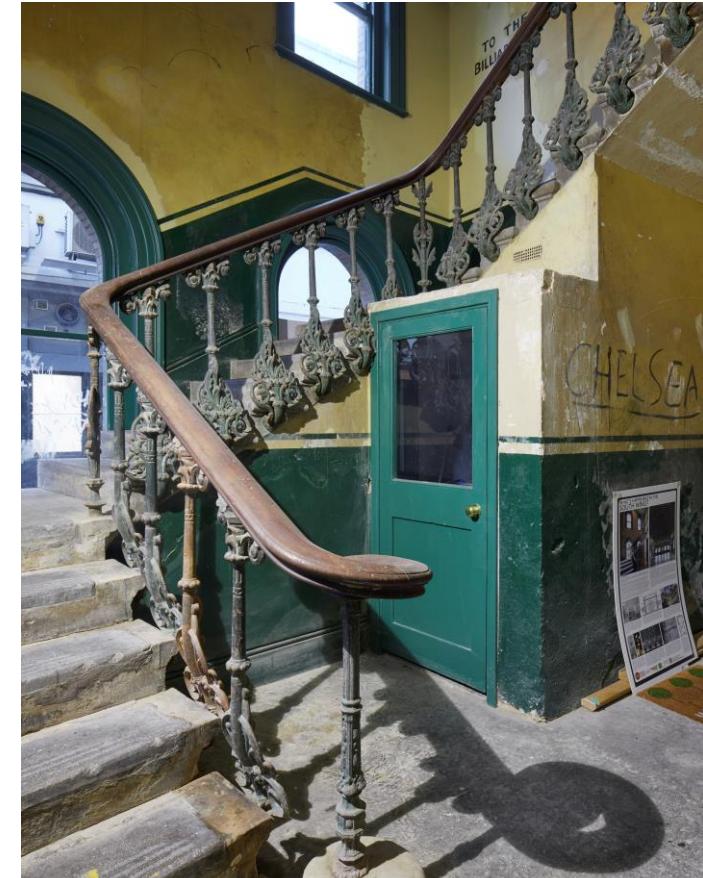
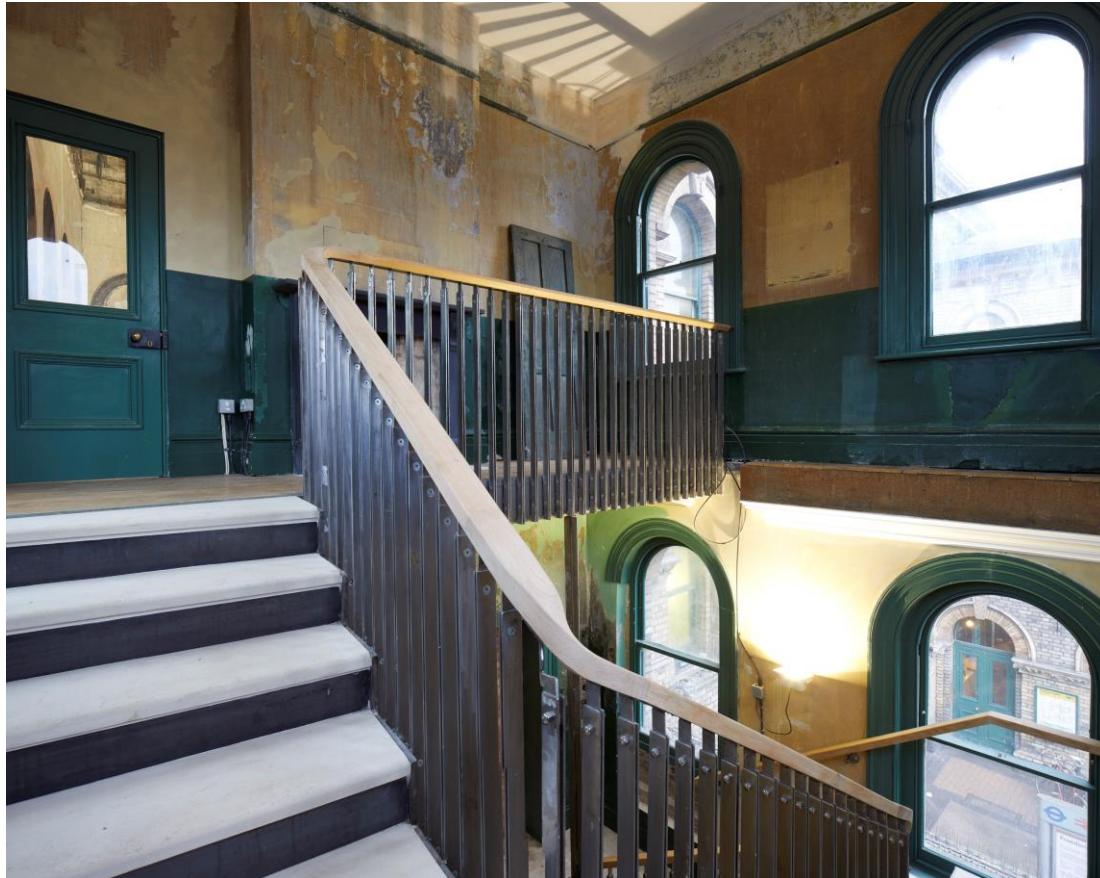
1 copy to 2 S. 24-6-85
- 2nd Engineer 9-8-85
- 3rd Engineer 9-8-85
- 4th Engineer (unexecuted) 9-8-85
- L. & G. 6-12-85

NED. NO. 26768

Peckham Rye— Southern Railway Architects circa 1935



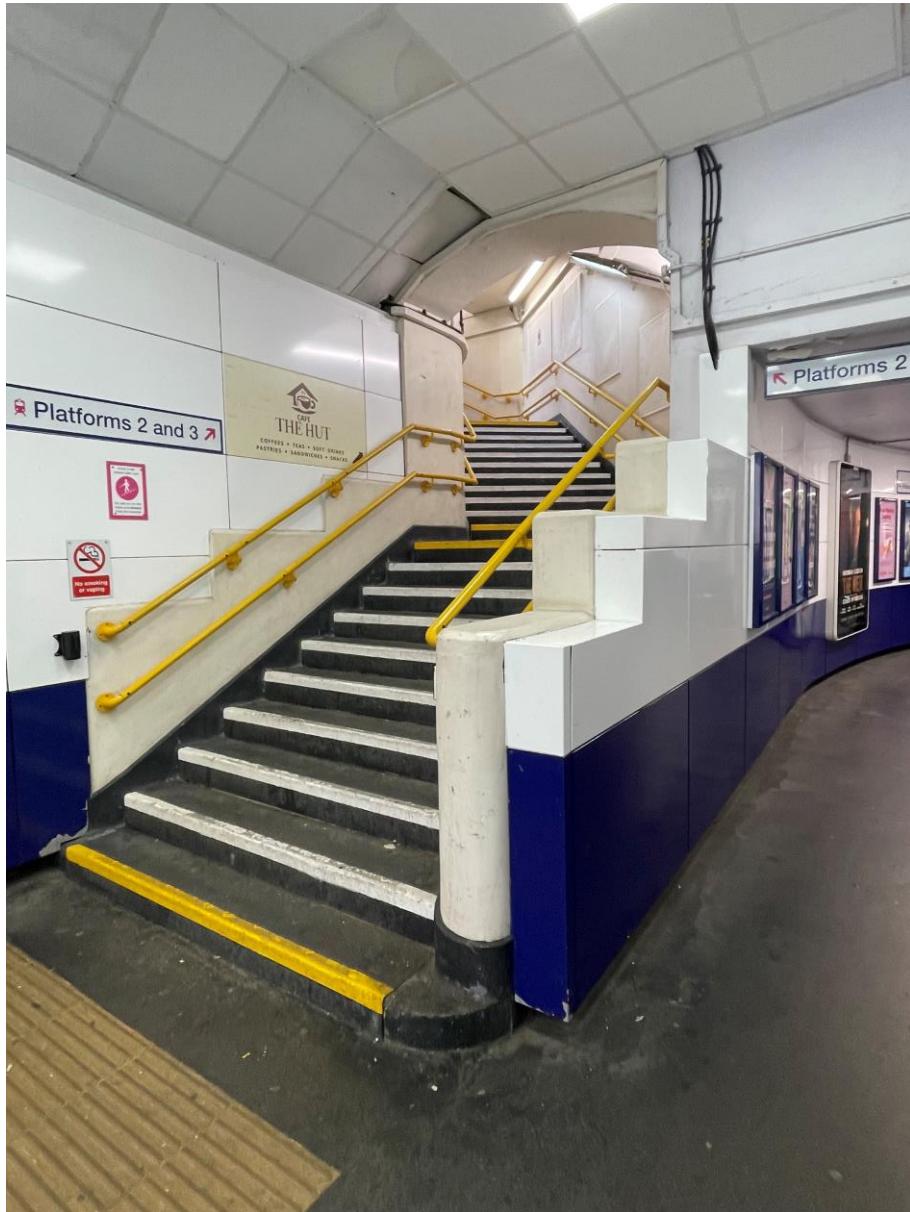
Elephant & Castle Station – Southern Railway Architects circa 1930 and 1950



Richmond Station – Southern Railway Architects 1935 – Stair detailing



Elephant & Castle Station – Southern Railway Architects



Elephant & Castle Station – Southern Railway Architects

TOT. WHITE CLASS. BIG
 BEN TAC. CLAMPING AGAINST
 CAST IRON SKELETON. PALACE
 OF WESTMINSTER. FALCON
 BIRDS OF PREY. MIKE McCANN
 THE KEEPER OF THE CLOCK.
 MARK COLLINS HISTORIAN AND
 LIBRARIAN.

RE-DISCOVER
 THE HIDDEN
 1930'S TILED POSTER
 PANEL SURROUNDS
 NOW HIDDEN

WORRIED ABOUT WATER?
 A GIANT MATWEI. ABOUT
 HAWK:

FLOOR
 CONSIDER A NEW FLOOR
 FINISH. THE PRESENT SURFACE
 IS LIFE-EXPIRED

NICE DRAWING OF
 ENTRANCE NR. 25030
 HORIZONTAL SHUTTER. NICE
 NICE DETAILS TOO.

1388' PLATFORM CANOPY DRAWINGS
 S.R. WATERLOO STATION. JULY 1947
 E.L. W.; M.M.C.; C.E.H.; APP. H.A.
 OCTOBER 1947.

TOM AND BARBARA. THE GOOD LIFE. THE
 21-



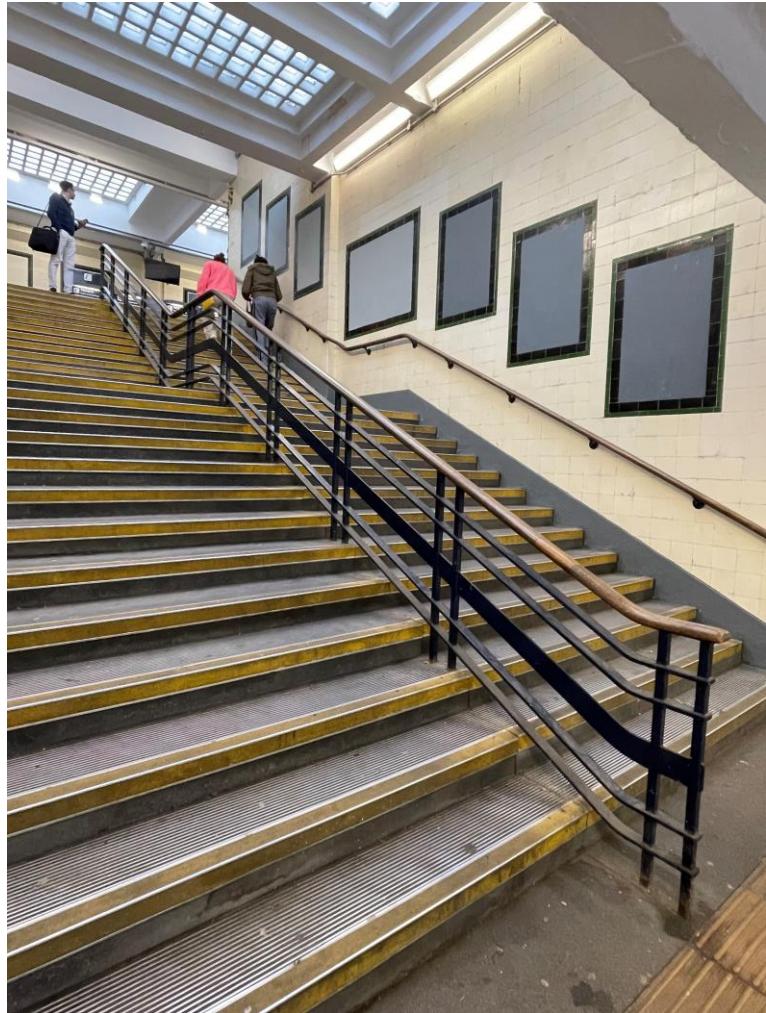
KENDALL FAMILY. INDIA.

COLTER AND PISSARO. 16' C.
 CHARLES (V) AND ISABELLA. FAST. FIRM LOVE

GRANITE FLOOR AND 6" GRANITE SKIRTING (1949. DRAWING. G.E. WALLIS & SONS
 THE RAILWAY EXECUTIVE. 4" 6" TO ONE INCH)

29 SEPTEMBER 1949) 30 SEP. 1949 WT

Surbiton Station – Southern Railway Architects 1935 – Stair detailing





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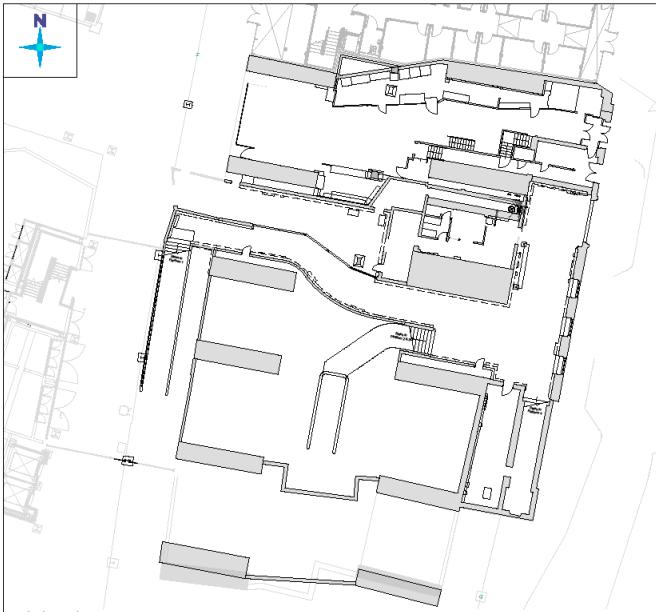


A THOUGHTFUL
TWO-TONE
PAINT SCHEME
COULD SUBTLY
ACCENTUATE THE
PROGRESSIVE FERRO
CONCRETE CONSTRUCTION
OF THE STATION

REVEAL THE
MID-20th CENTURY
POSTER PANEL TILED
SURROUNDS WITH GREEN
AND BLACK TILES

CONSIDER A TWO
TONE PAINT SCHEME
PERHAPS THE MID-
20th CENTURY PAINT
SCHEME.

Elephant & Castle Station



Existing Plan



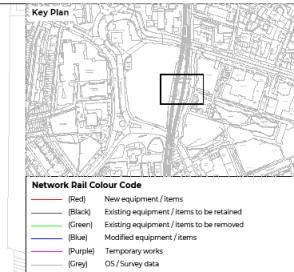
Aspect 0 - Ticket Office Relocation and Demolition of Town Centre Entrance Box



Aspect 1 - Installation of Town Centre Temporary Entrance and Gatedines



Aspect 2 - Station Enhancement Scheme



Network Rail Colour Code

- [Red] New equipment / items
- [Black] Existing equipment / items to be retained
- [Green] Existing equipment / items to be removed
- [Blue] Modified equipment / items
- [Purple] Temporary works
- [Grey] OS / Survey data

Drawing Notes

1. Do not scale from this drawing.
2. This drawing must be read in conjunction with all specifications, instructions, civil, structural, mechanical and electrical drawings.
3. Figured dimensions only to be used.
4. All dimensions are in millimetres unless otherwise stated.
5. All dimensions to be checked on site before commencement of works.
6. Any differences or discrepancies must be referred to the contract.

P02	A1102	Preliminary Issue	DR	AT	CL
P01	18/25	Preliminary Issue	DR	AT	CL
Rev Date		Description of Revisions	Drawn	Child	Appr
Status	Preliminary Issue				



Project
Elephant & Castle
Accessibility Improvements

Drawing Title
Station Improvements Phasing

Designed	A.Thompson	Signed	Date	1/8/25
Drawn	D.Rees	Signed	Date	1/8/25
Checked	A.Thompson	Signed	Date	1/8/25
Approved	C.Lamb	Signed	Date	1/8/25
Scale(s)	1:200	ELR & Mileage	HHH & 1m47ch	
Alternative Reference	Sheet of			
Drawing Number	184655-IVU-ZZ-ZZ-DRG-EAR-000001			
	Revision P02			

Elephant & Castle Station



Elephant & Castle Station



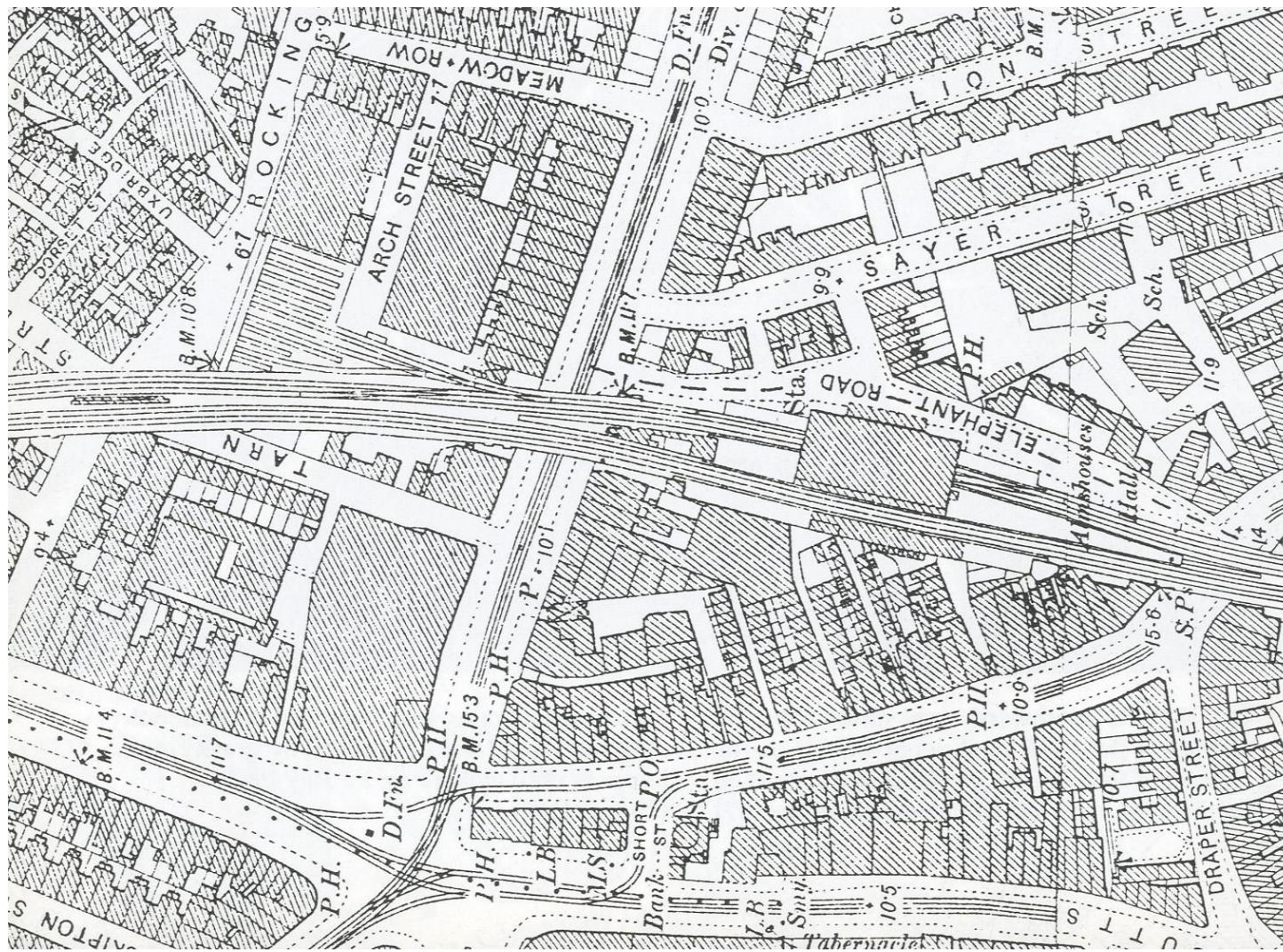
Elephant & Castle Station



Elephant & Castle Station



Elephant & Castle Station – 1895







In the days when these buildings were covered with a century's dirt, it is perhaps not surprising that they were not highly regarded either by the public or by railway management. Little sense of bright colour is evident in this late 1960s view of Herne Hill! The SR advertising continues to stress the twin aims of the LC&DR's Metropolitan Extensions. *Jim Connor*

Herne Hill c.1900, showing the south end of the building, which was not given any great architectural treatment, save for the bands. At first glance the 1862 LC&DR valancing of the roadside canopy might be taken as the standard SER 1890s/1900s type.

J.E. Connor collection



This illustration from *The Building News* 9.1.1863 is the only known view of any of the stations when new, and shows how the original 'Taylor' tiles (for which the architect was well-known) particularly added distinction to the roofs here, more so perhaps than at Penge East and Dulwich (pp. 316/7). Particularly conspicuous is the angled cast ironwork at the corners, which survives at Penge East, Bat & Ball, and Clapham (p.316). Note that the two first-floor lancet windows in the tower were originally glazed, as were some of those at the top of the tower. The full-length roadside canopy lasted only until the early C20, as it had already been cut back to the middle third as now by the 1916 OS. All other details here are unchanged in 2013, save for the disappearance of the additional exit stairs at left.

The Building News described the station as 'spacious and convenient....and of the very best quality....an unusual amount of decorative taste has been displayed'. It must be remembered that 1862/3 was a relatively early date for such commodious buildings and lengthy canopies at suburban stations, although they were to become seen as the norm for new suburban stations by the 1870s.

The decorative bands, which are the same on the tower as on the main frontage section, are shown correctly (save for one error):

Limestone only, under eaves: Moulded red glazed terracotta plus limestone below, at head of first floor windows: Limestone only, at middle of first floor windows: Moulded red glazed terracotta plus limestone below, at first floor window sill level: Limestone only, under canopy: Moulded red glazed terracotta plus limestone below, at head of ground floor windows: Limestone only, at middle of ground floor windows: Limestone only, at ground floor window sill level (wrongly, it would appear, shown here as moulded red glazed terracotta plus limestone below).



Now that most of the remaining buildings have been cleaned up, it is much more evident why they were seen as striking by contemporaries. This is the front of the eastern pavilion at Penge East, and shows features that were standard at all the stations (with exceptions at West Dulwich, opposite below). London stock brick is used principally, with red brick for the dentillated courses under the eaves (or across the gable end here). There are multiple moulded glazed terracotta bands of reddish hue, each with a flush white limestone course below; plus other bands consisting of a flush white limestone course only. The window and door openings have red brick pointed arches with white limestone imposts and keystones, each with a trefoil decoration; and an outer arch of blue engineering brick. The circular (oeil-de-bœuf) openings are only found in the gable ends at Penge East and Bat & Ball.



The Booking Hall doorways here at Penge East and most of the other stations (cf Herne Hill p.314) have these higher pointed arches, with timber 'plate tracery' above the door.



Detail of the upper part of the tower at Herne Hill (which housed a water tank). If one looks carefully at the brickwork of the second and fourth openings, which were originally glazed (open?), it is seen to be different. There is no trefoil decoration in the impost stones of these arches. A raised limestone band runs across the foot of the blind openings. Below that is a moulded red terracotta plus limestone band; below which the bands are described at p.313. Between the pointed arches and the red brick dentillation under the eaves, there is a single proud course of ordinary stock brick.

Detail of the frontage at West Dulwich, which is very much the 'odd man out' amongst these stations. It is built principally of this cream brick instead of stock brick. The window and door openings are *round-headed*, and specially-shaped red bricks are utilised to create the pointed arch above, requiring a large keystone with an extended 'stem and leaf' decoration below the trefoil. The two red moulded glazed terracotta bands are much smaller, having the depth of an ordinary brick course only, and there are no white limestone bands – only this one-course band of red brick halfway between the red moulded courses. Unusually there is also a red brick plinth.





Left: The rear of the Clapham building has standard type window and door openings. The dentillation under the eaves is virtually the same as on the Herne Hill tower. Note that here there are some red moulded courses without a limestone course below.



Right: Silhouette of one of the angled pieces of cast ironwork at the roof corners at Clapham.



At West Dulwich the line is on embankment, with the single-storey building at street level below. The significant differences between the details of this building and the others have been noted already at p.311. The 'Taylor' tiles here lasted into the 1930s, as seen here. The platforms at West Dulwich had timber shelters (now gone), not standard 1862/3 type canopies.

Courtesy John Minnis



A more recent view of West Dulwich after cleaning of the brickwork.

Jim Connor



Penge retained its 'Taylor' tiles until the early C20. The date of removal of the roadside canopy is unknown, as the OS maps are indeterminate on this point. Hiding as it did the decoration of the middle part of the frontage, it is doubtful if it improved the appearance of the building. The two canopy columns had wooden brackets in the same spirit as the Herne Hill canopy brackets.

J.E. Connor collection



The same scene in the early 1960s. The house entrance acquired a small canopy on removal of the main canopy. At left is the goods shed which, like Clapham's, was built in the same style as the station building. It has since been demolished. (The only other station to have a goods shed was Camberwell but no photographs of it are known). Note the chimney style used for all these stations.

J.E. Connor collection

Elephant & Castle Station



Elephant & Castle Station – Early 20th century

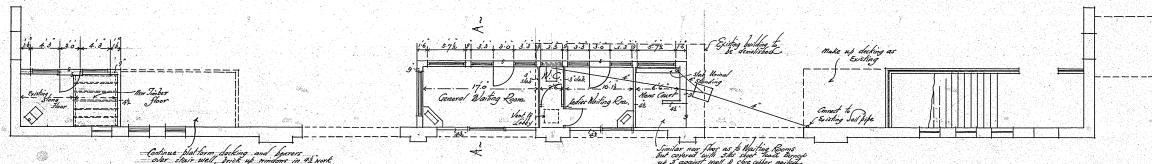


S. R. ELEPHANT & CASTLE STATION
PROPOSED PLATFORM OFFICES ON 'UP' MAINSIDE

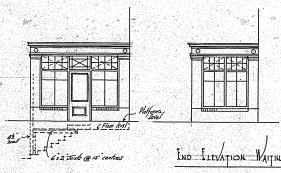
4 Cries W. Davis



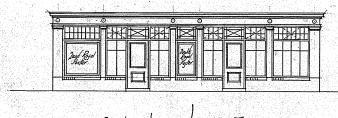
SZ7105461



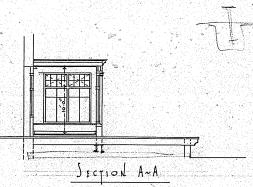
PLAN OF NEW BUILDINGS ON 'UP' MAIN SIDE. $\frac{1}{8}$ SCALE



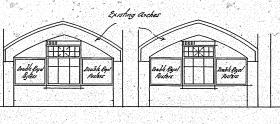
END ELEVATION WAITING R.



Elevation of Waiting Room

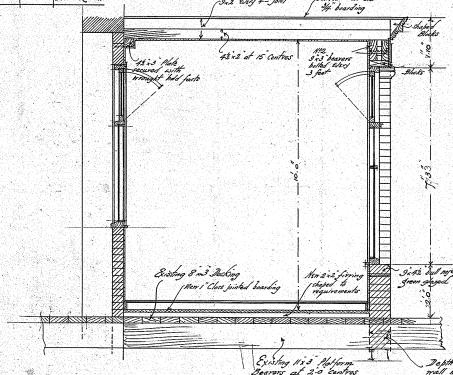


Section A~A

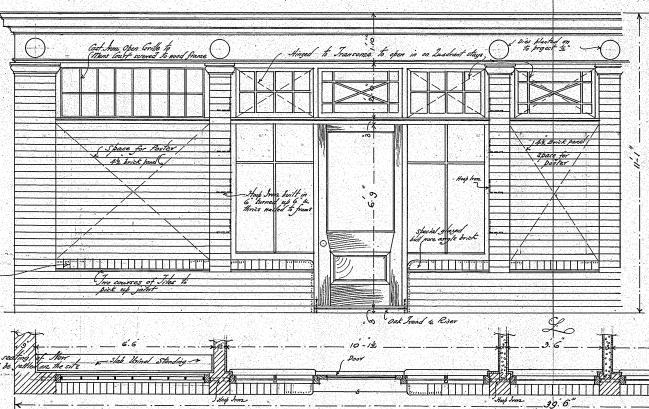


BACK ELEVATION OF WAITING RNS.

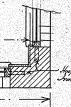
Note: The stock facing bricks and
Green glazed bricks to plinths are to be
similar to those recently used on Down side.



220891

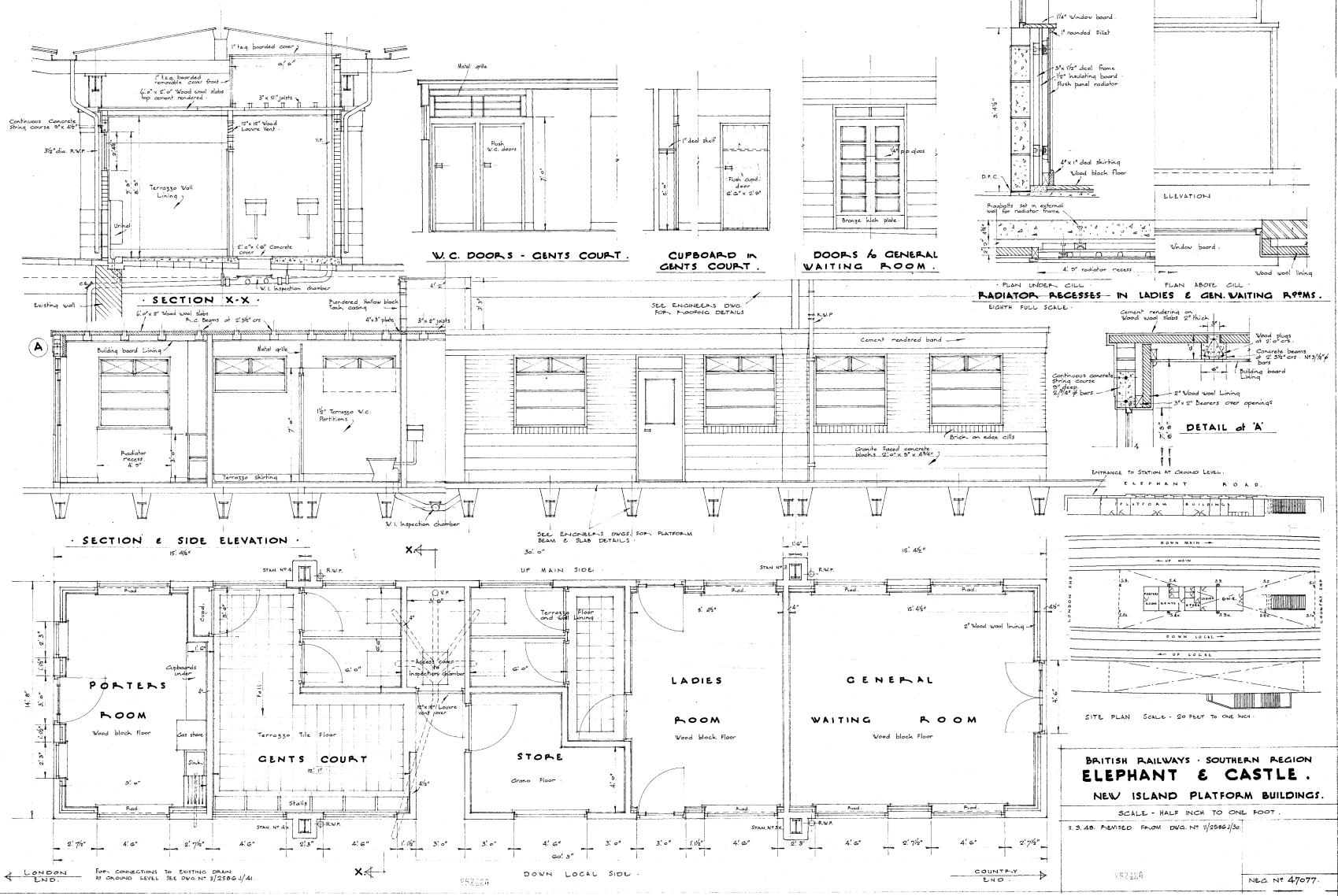


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NEG. NO 18206



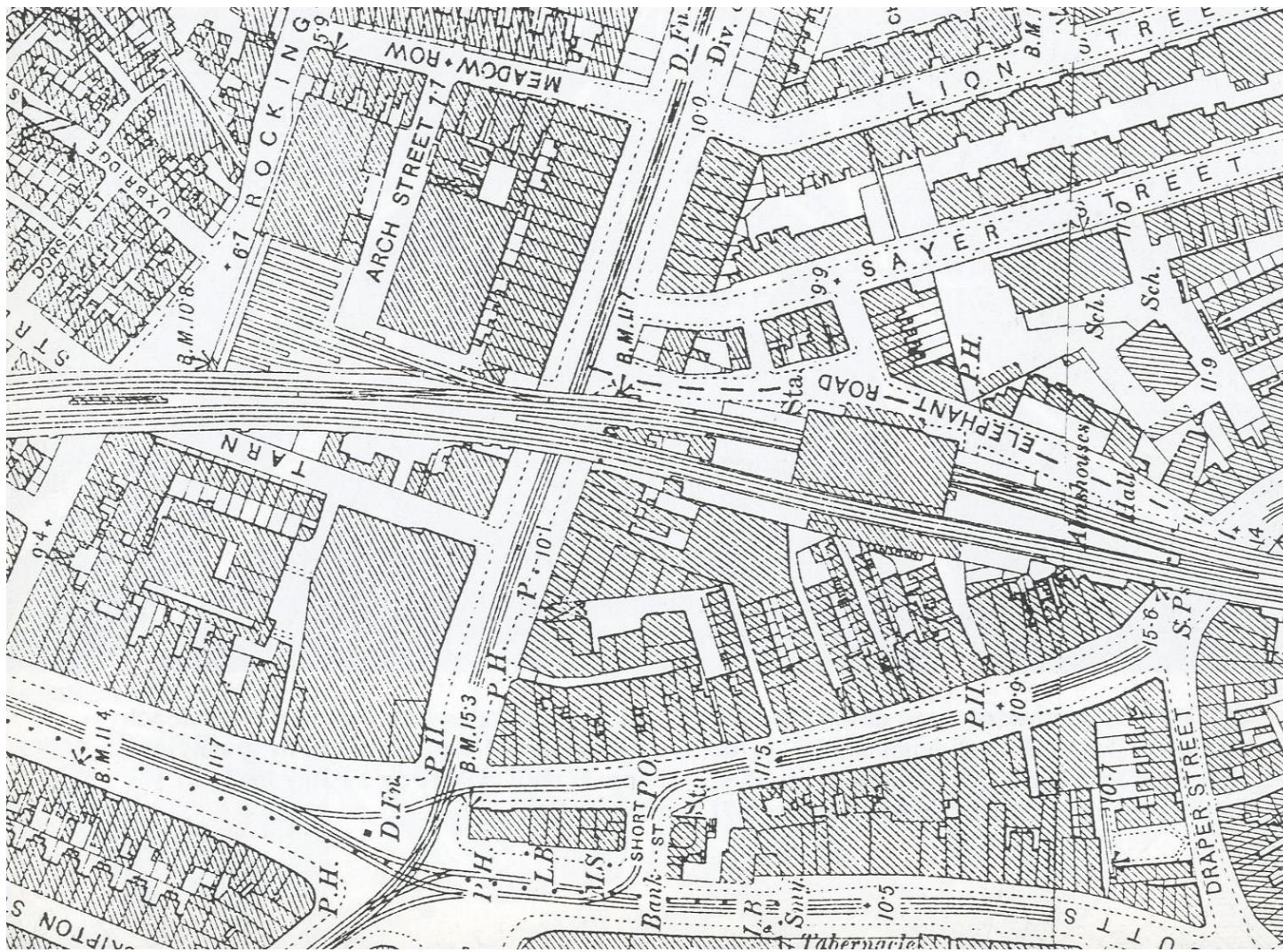
Elephant & Castle Station – Post war



Elephant & Castle Station – Post War



Elephant & Castle Station – 1895



Elephant & Castle Station
Proposed Front (East) Elevation
I:150 @ A3



0 1 2 3 4 5 6 7 8 9 10 METERS

Elephant & Castle Station

DESIGN STUDY

Copyright Benedict O'Looney Architects 2021. Do not scale from these drawings.
Verify all dimensions on site and report any discrepancies to the architect / contract administrator immediately.
All structural elements to be designed by the structural engineer.

SCALE: I:150 @ A3
I:75 @ A1

DRAWING: EAC_p_150_e1
DATE: 27/03/23

Benedict O'Looney Architects
56 Pennethorne Road Peckham
SE15 5TQ t: 0207 732 9713

Elephant & Castle Station
Proposed Front (East) Elevation
 Southern portion

General notes

The 1920's pre-cast ferro-concrete platform parapet walls are to be clean and checked for defects. Keim lime wash to be applied to create a uniform finish.

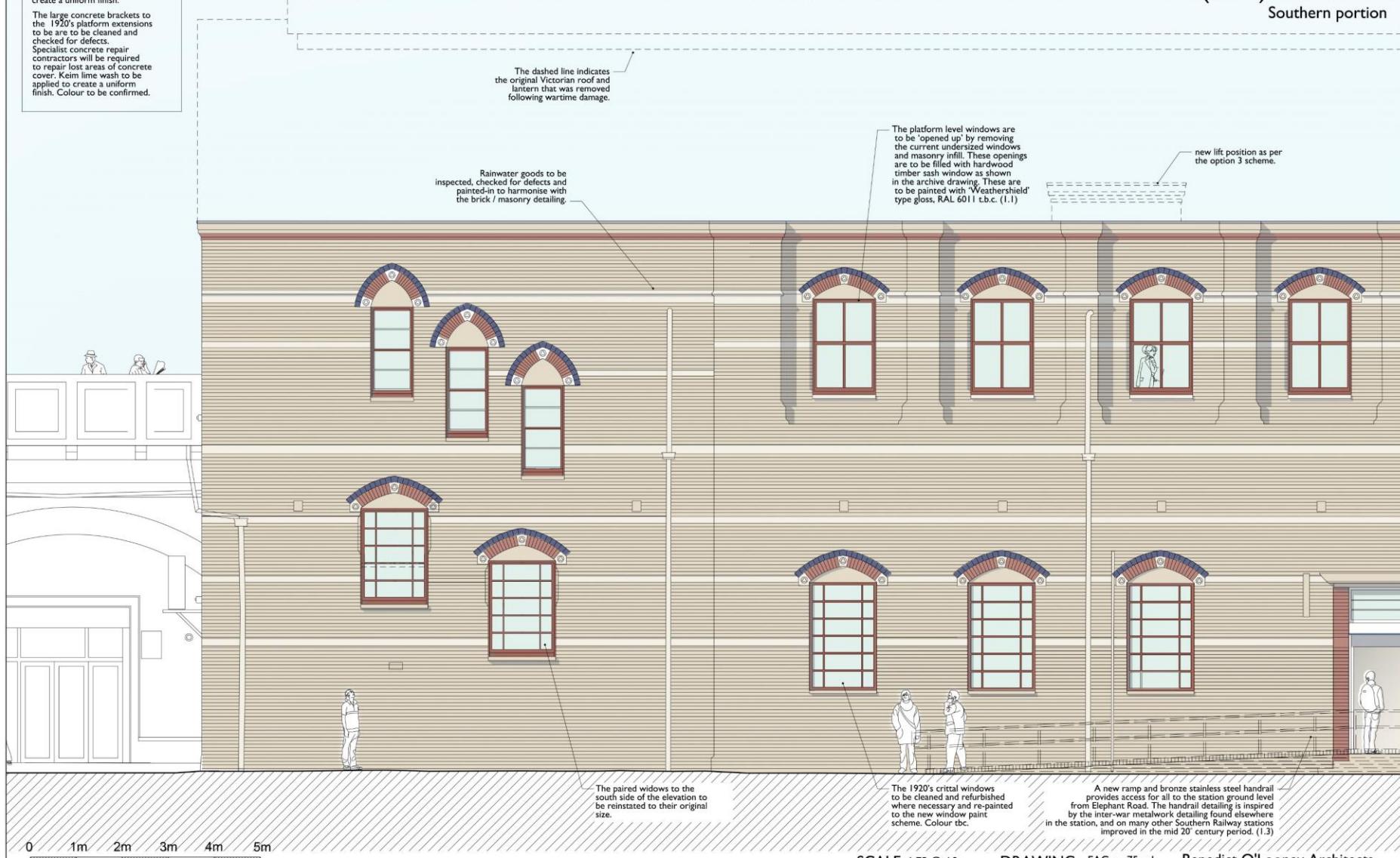
The large concrete brackets to the 1920's platform extensions to be are to be cleaned and checked for defects. Specialist concrete repair contractors will be required to repair lost areas of concrete cover. Keim lime wash to be applied to create a uniform finish. Colour to be confirmed.

The dashed line indicates the original Victorian roof and lantern that was removed following wartime damage.

Rainwater goods to be inspected, checked for defects and painted-in to harmonise with the brick / masonry detailing.

The platform level windows are to be 'opened up' by removing the current undersized windows and masonry infill. These openings are to be filled with hardwood timber sash window as shown in the archive drawing. These are to be painted with 'Wethersfield' type gloss, RAL 6011 t.b.c. (I.I)

new lift position as per the option 3 scheme.



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 Verify all dimensions on site and report any discrepancies to the architect / contract administrator immediately.
 All structural elements to be designed by the structural engineer.

SCALE: 1:75 @ A3

DRAWING: EAC_p_75_e1
 DATE: 27/03/23

Benedict O'Looney Architects
 56 Pennethorne Road Peckham
 SE15 5TQ t: 0207 732 9713

Elephant & Castle Station

Elephant & Castle Station
Proposed Front (East) Elevation
 Northern portion

General notes

The 1920's pre-cast ferro-concrete platform parapet walls are to be clean and checked for defects. Keim lime wash to be applied to create a uniform finish.

The large concrete brackets to the 1920's platform extensions to be are to be cleaned and checked for defects. Specialist concrete repair contractor to be engaged to repair lost areas of concrete cover. Keim lime wash to be applied to create a uniform finish. Colour to be confirmed.

new lift position as per the option 3 scheme.

The platform level windows to be 'opened up' by removing the current undersized windows and infill to be replaced by a timber sash window as shown in the archive drawings.

New sash windows to be painted. Suggested colour oxide red to compliment the brick work.

The dashed line indicates the original Victorian roof and lantern that was removed following wartime damage.

Rainwater goods to be inspected, checked for defects and painted-in to harmonise with the brick / masonry detailing.

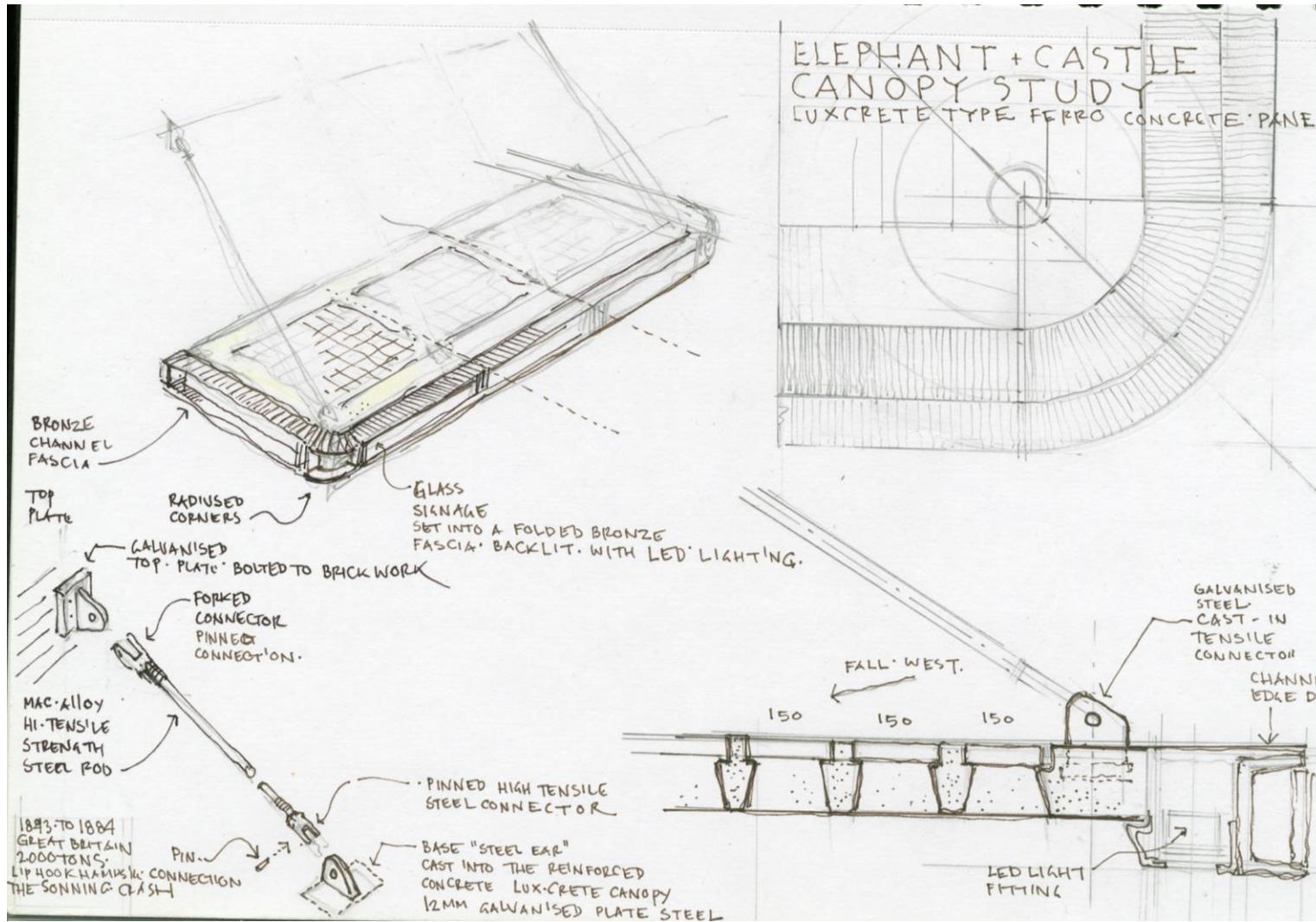
The triplet window to the north part of the facade to be cleaned and new period-style hardwood windows are to be introduced taking their cue from archive drawings and photographs.



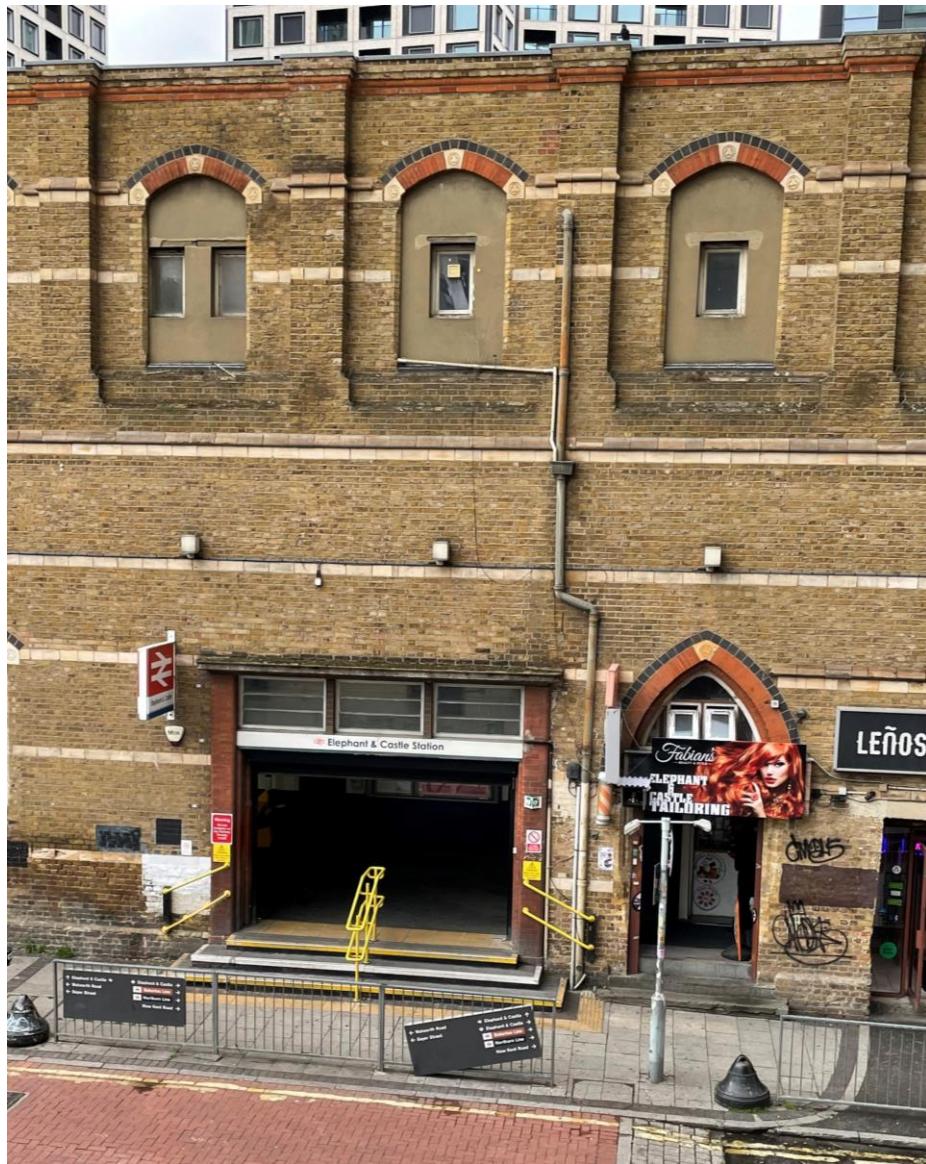
Elephant & Castle Station – Southern Railway Architects



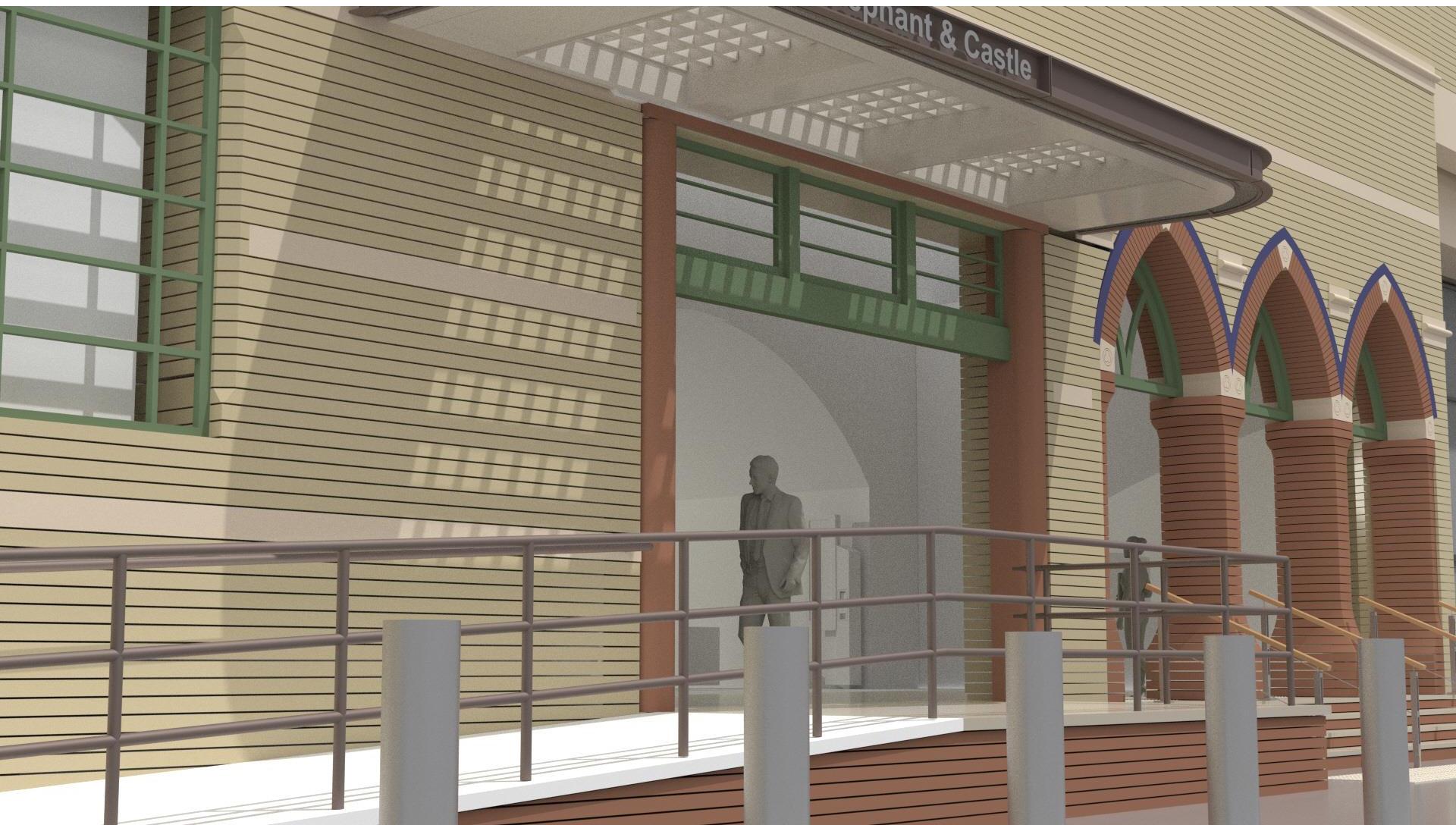
Elephant & Castle Station – Southern Railway Architects



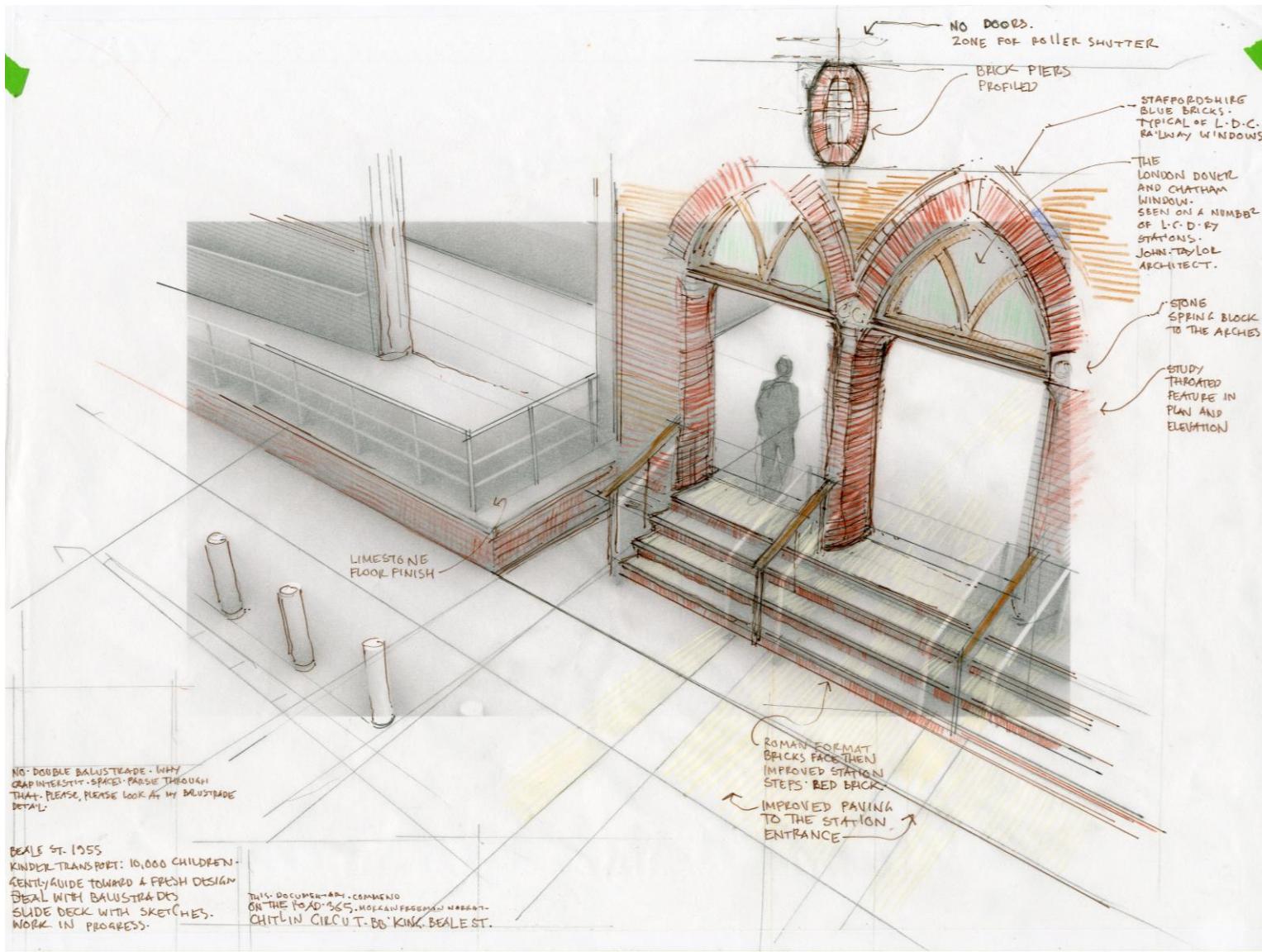
Elephant & Castle Station – LC&D rwy. & Southern Railway Architects, 1928 / 1950



Elephant & Castle Station

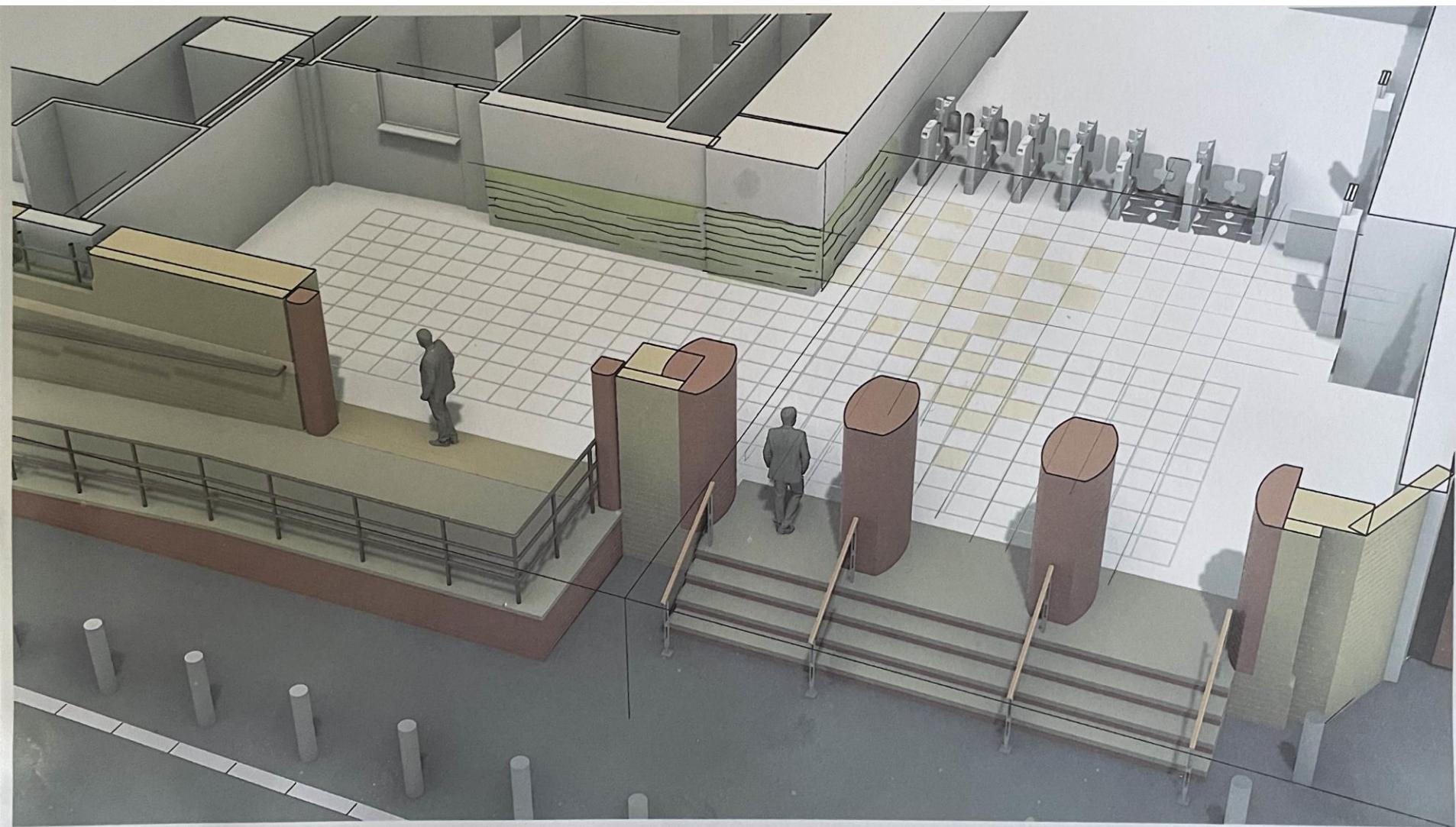


Elephant & Castle Station – Elephant Road Designs

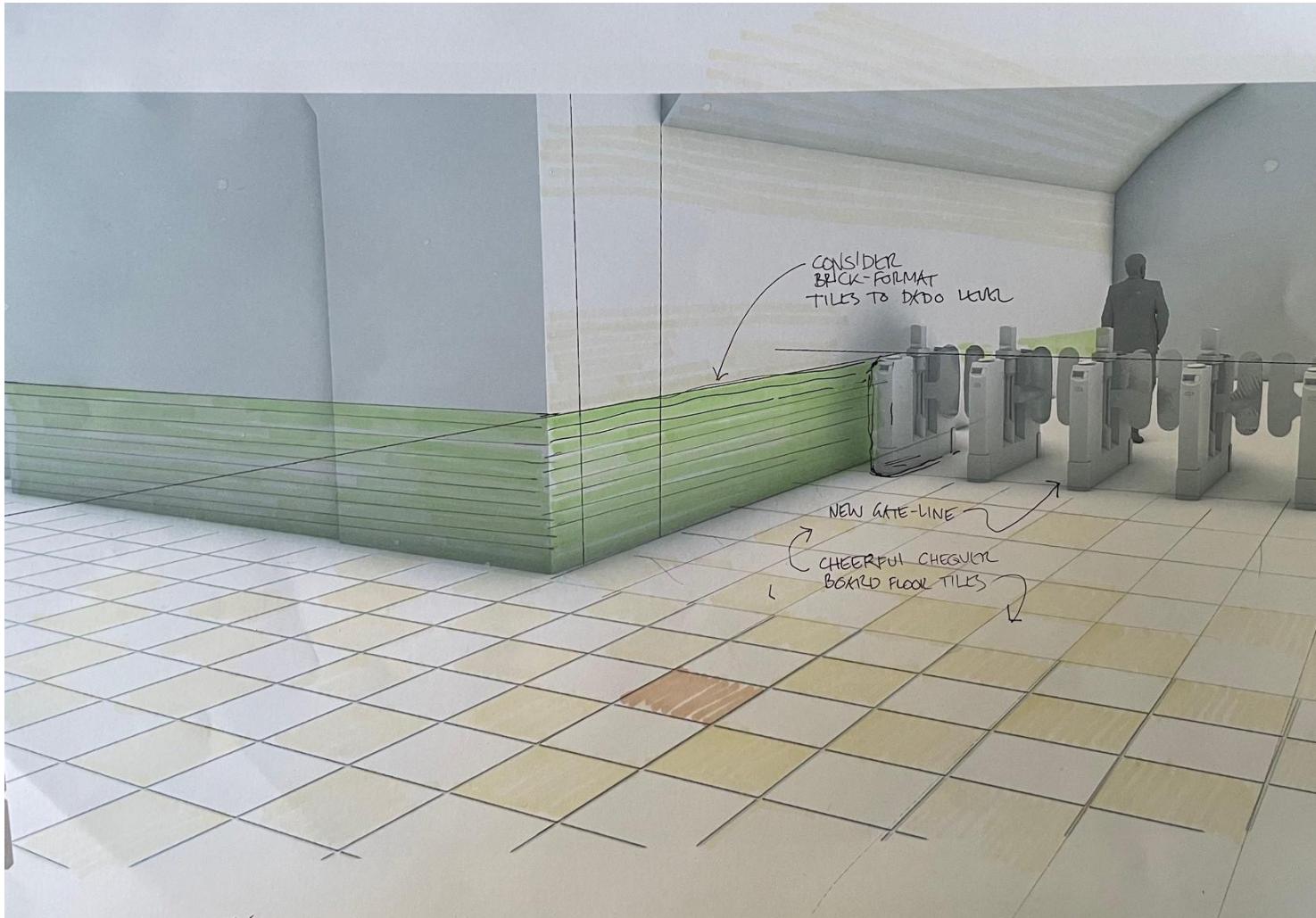


Elephant & Castle Station – Southern Railway Architects





Elephant & Castle Station – design studies



Main Historical Façade Entrance

3D model Overview



Main Historical Façade Entrance

3D model Overview



Main Historical Façade Entrance

Overview



2

Upper window bays are flanked with columns. These are possibly the strongest architectural elements on the façade and have therefore been highlighted. Two iPro uplighters have been positioned to light both the column sides and the main brick façade. The cornice above will also receive light.

Art: BK25



1

Window reveals illuminated with Linealuce 27 wall grazing wide flood optics mounted onto the windowsill. Either 329mm, 625mm or 920mm in length, depending on the size of the window bay.

Art: UE28 / UE56 / UE84



Main Historical Façade Entrance**3D model Overview**

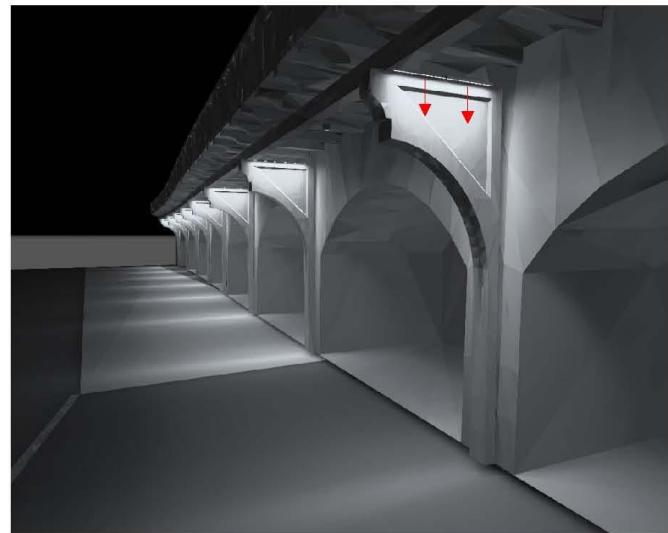
Pillars & Walkway

Overview

**1**

The upper portion of the concrete pillars are illuminated using 2Nr Linealuce 27 wall wash optic, linear down lights. These provide a strong vertical lighting element which is repeated the length of the station.

Art: UE84

**2**

A 155mm iPro flood light has been proposed, centrally above each arch. These provide the functional lighting over the pavement below.

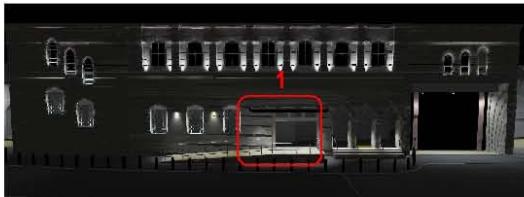
Light Level (pavement) = 50Lux 0.40Uo

Art: EP65 + BZ67 + BZ63



Main Historical Façade Entrance 1

Overview



1

Entrance 1 - illumination covering the top of the ramp.
Using Laser Blade 5 cell down lights mounted on the
underside of the canopy within the 'skylight element'.

Art: E886



Main Historical Façade Entrance 2

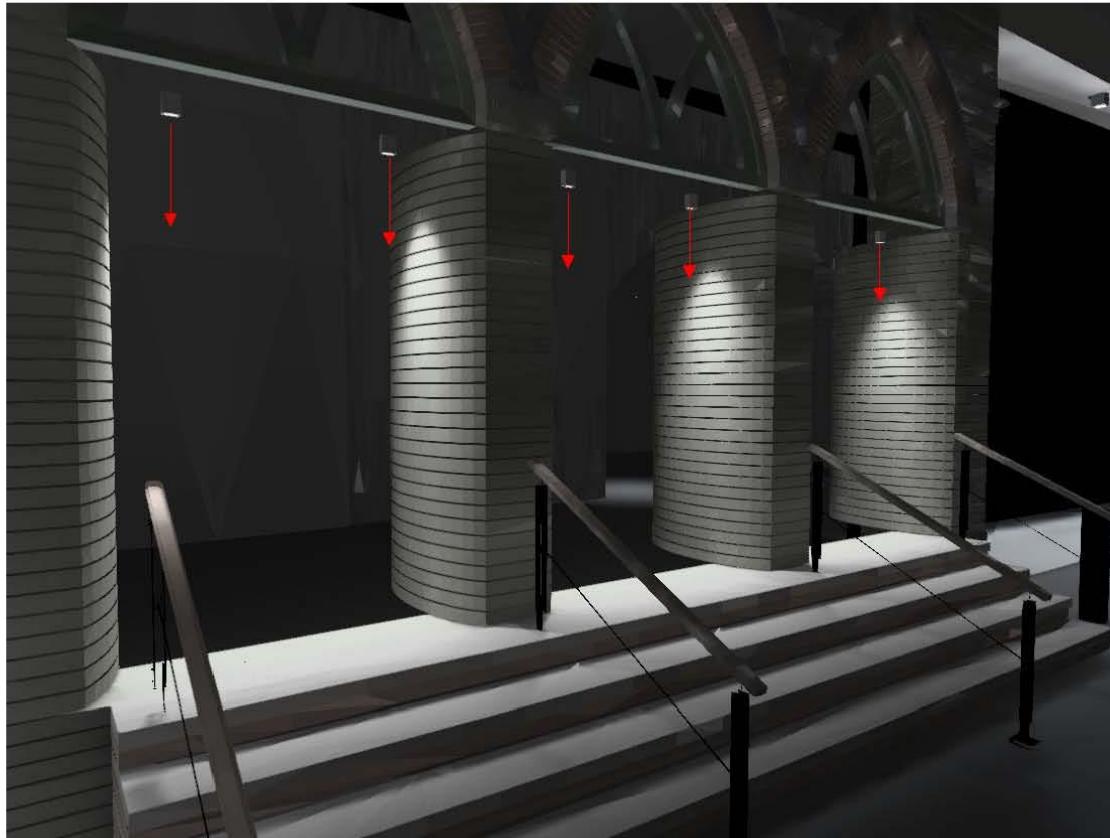
Overview



2

Entrance 2 - lighting over the entrance threshold and steps. Laser Blade in/out 2 cell down lights. 50-150Lux. Vertical lighting provided over brick columns to hi-light the entrance.

Art: E882



Main Historical Façade Entrance 3

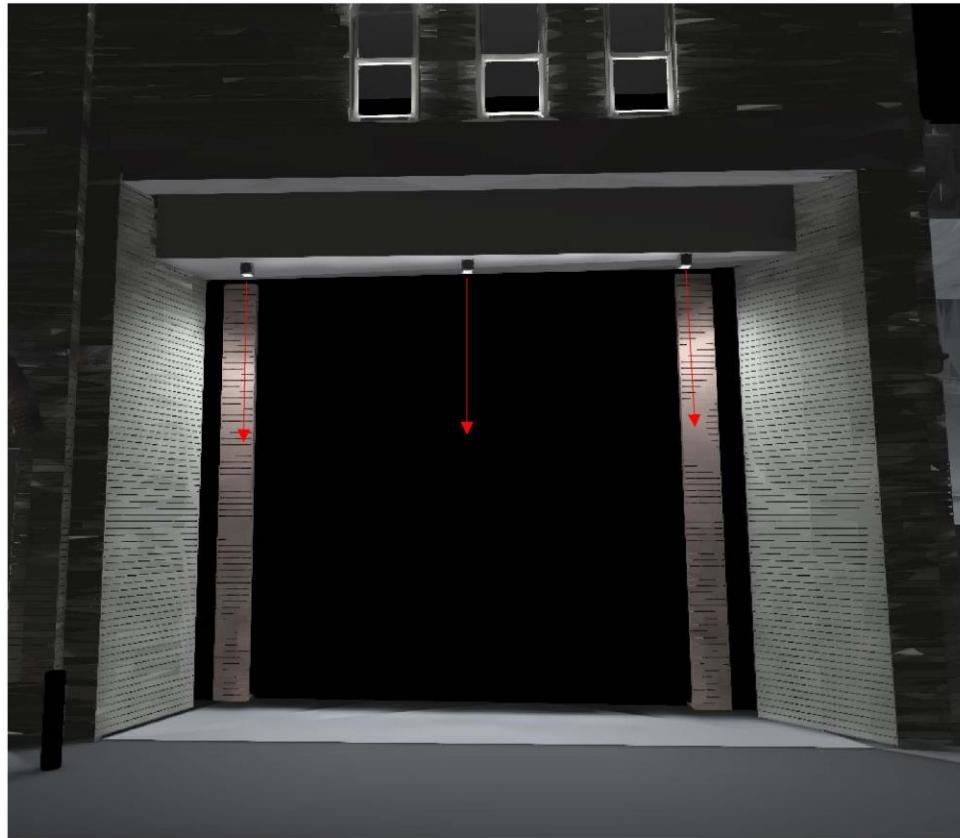
Overview



3

Entrance 3 - illumination over the threshold using three iPro surface mounted down lights. Again, attention has been made to make sure the vertical surfaces around the entrance are well illuminated. 100lux is provided over the pavement threshold area.

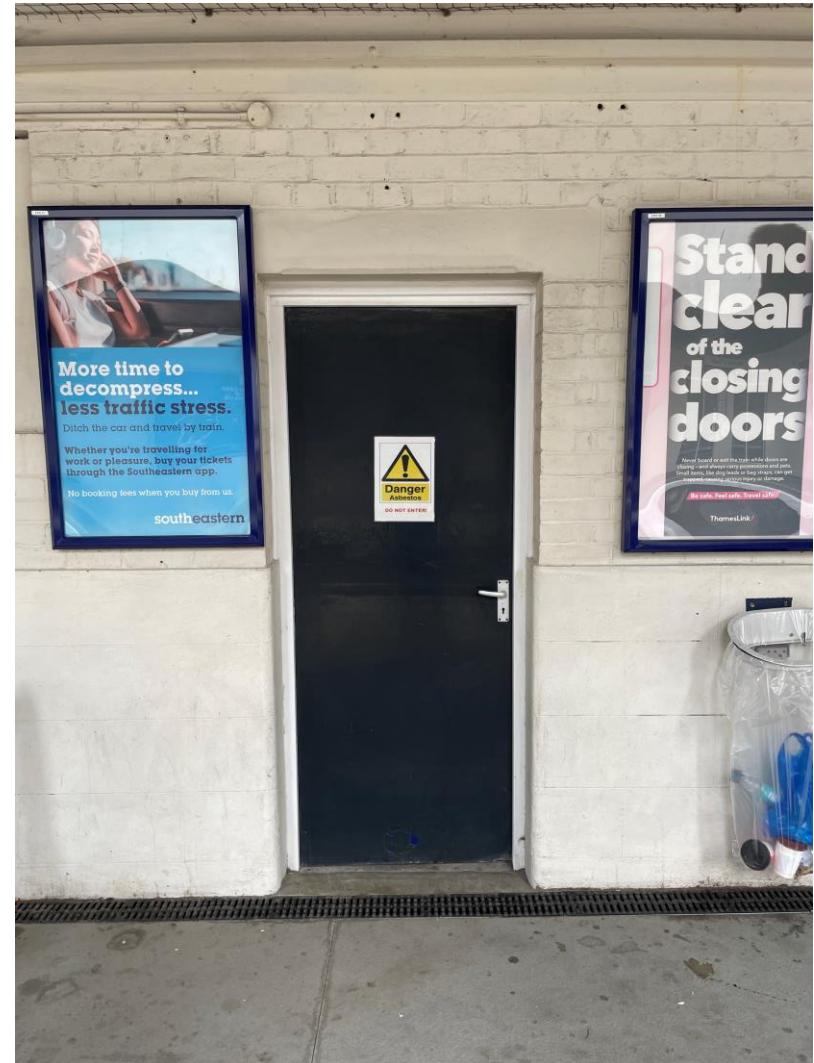
Art: EP65 + BZ67 + BZ63



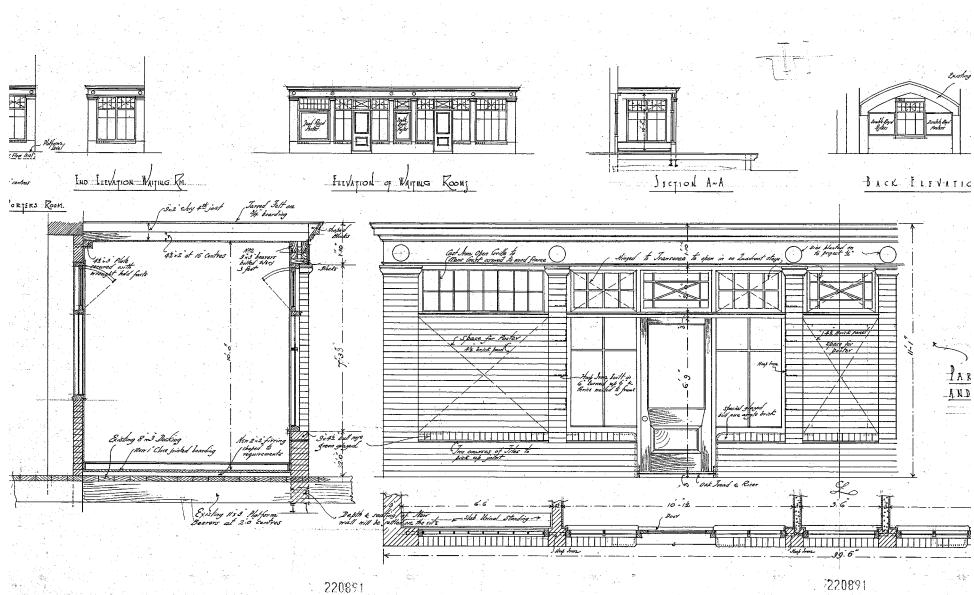
Elephant & Castle Station – Early 20th century



Elephant & Castle Station – Southern Railway Architects



Elephant & Castle Station – Southern Railway Architects





21:20 Sevenoaks
Thameslink service formed of 8 coaches. 1
3rd 22:20 Sevenoaks
21 min
On time
20:58:47

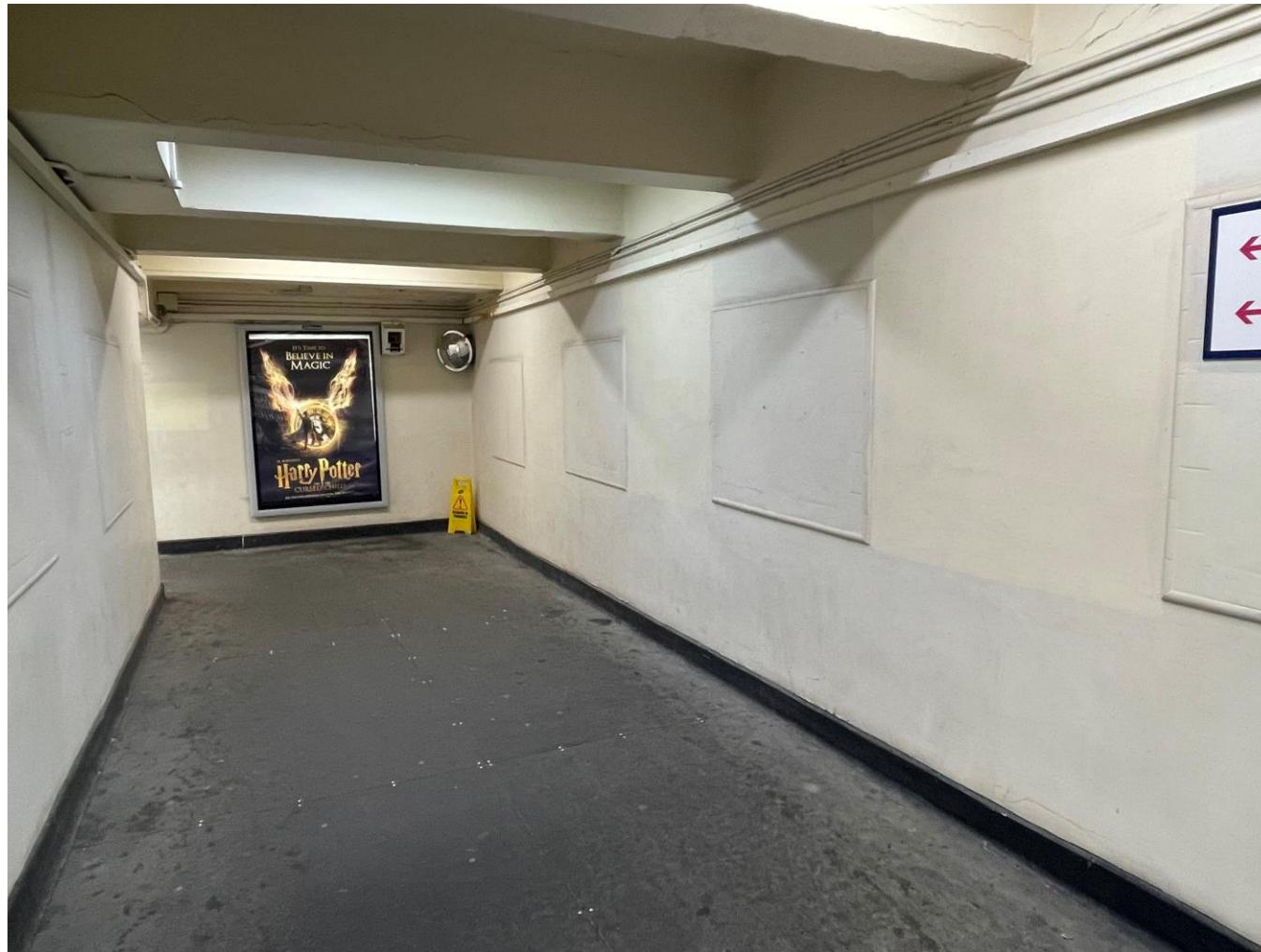


Way Out

Elephant & Castle Station – Southern Railway Architects



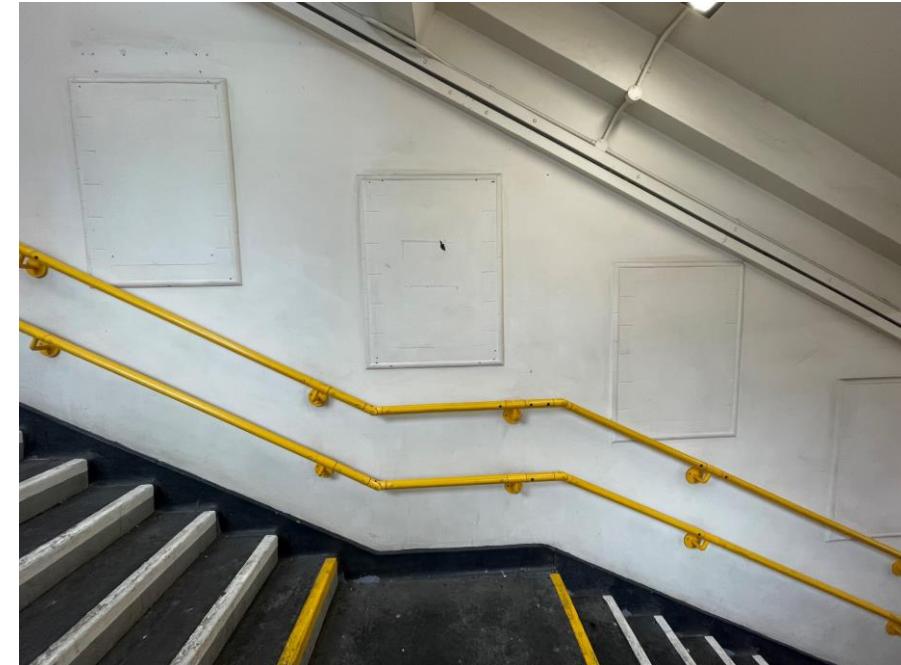
Elephant & Castle Station – Southern Railway Architects circa 1930 and 1950



Elephant & Castle Station – Southern Railway Architects circa 1930 and 1950

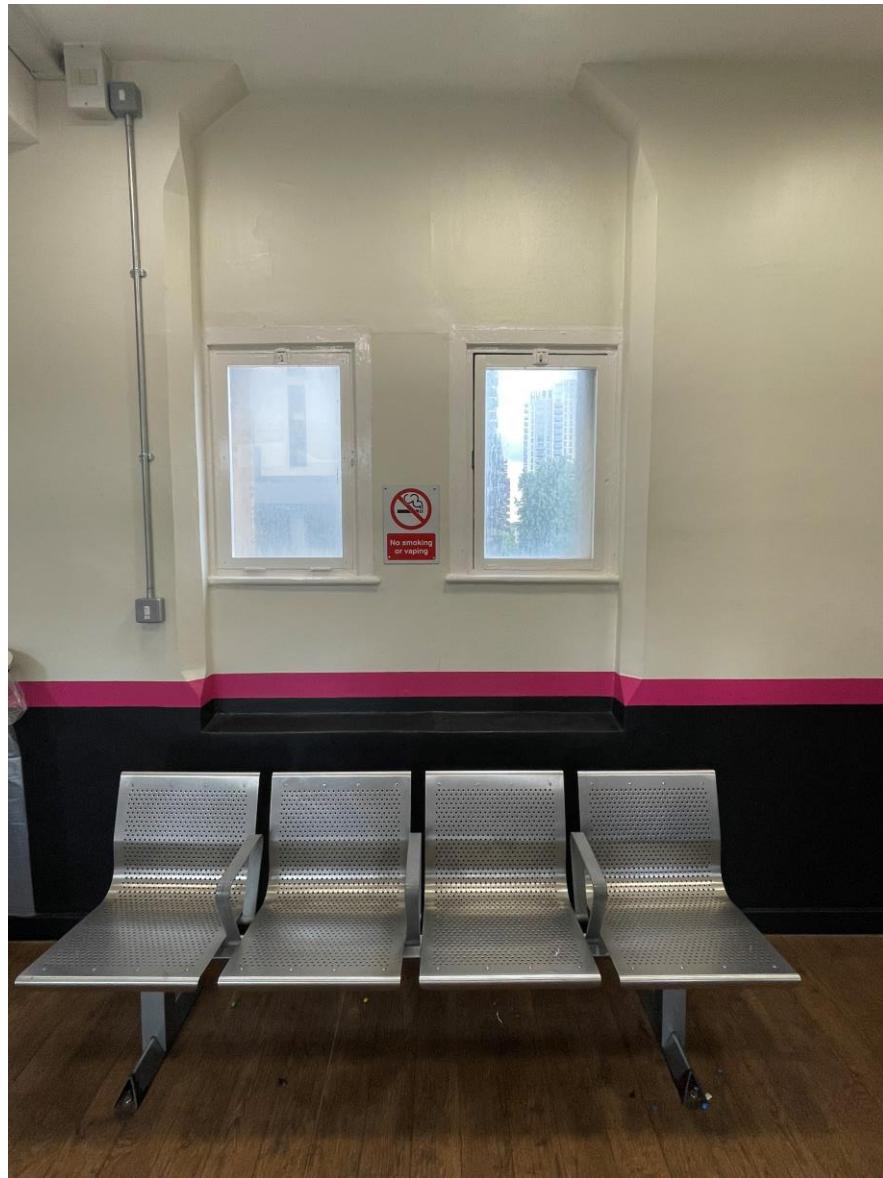


Elephant & Castle Station – Southern Railway Architects circa 1930 and 1950



Richmond Station – Southern Railway Architects 1935 – Stair detailing



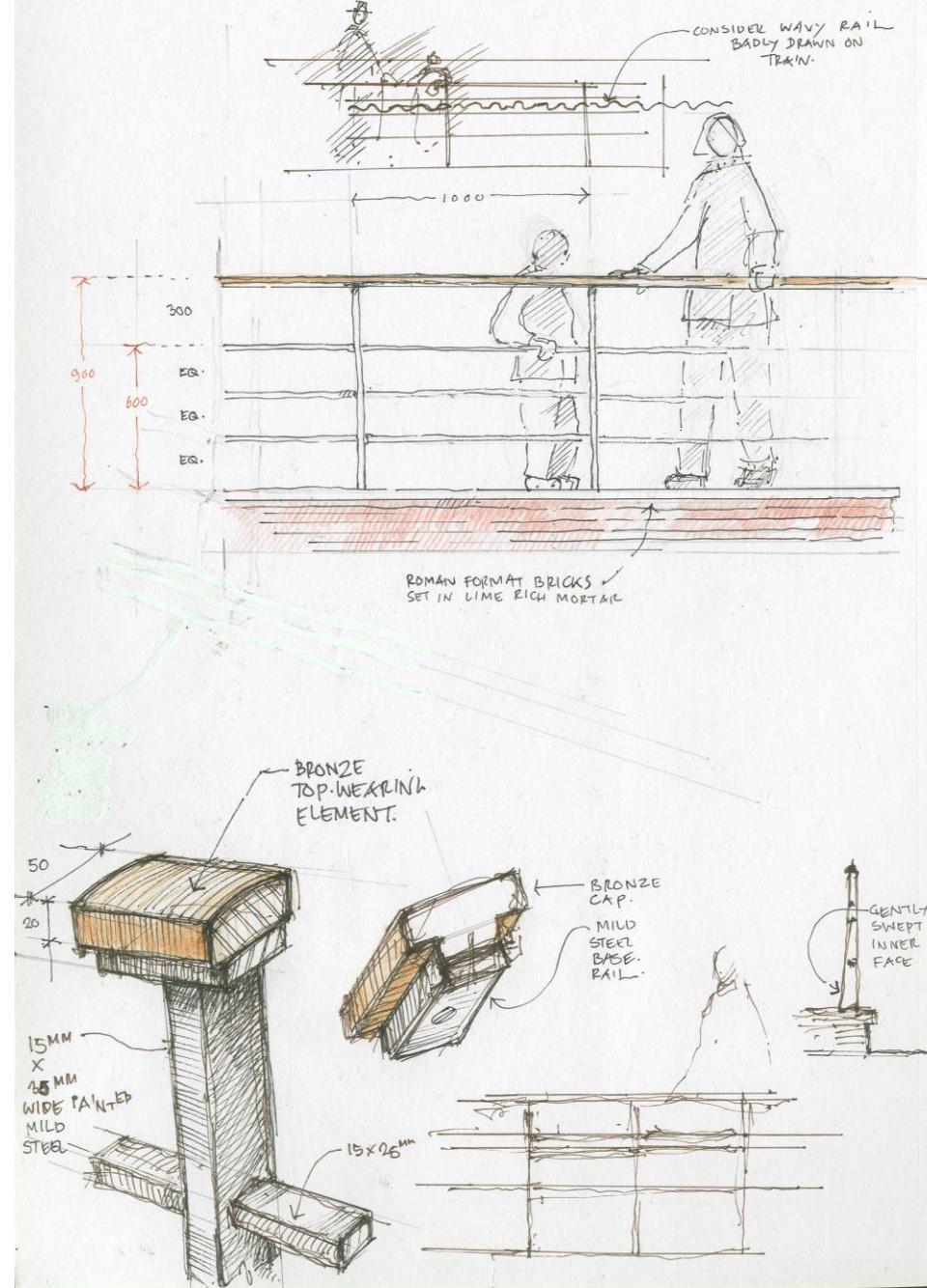


Elephant & Castle Station – Southern Railway Architects circa 1930 and 1950

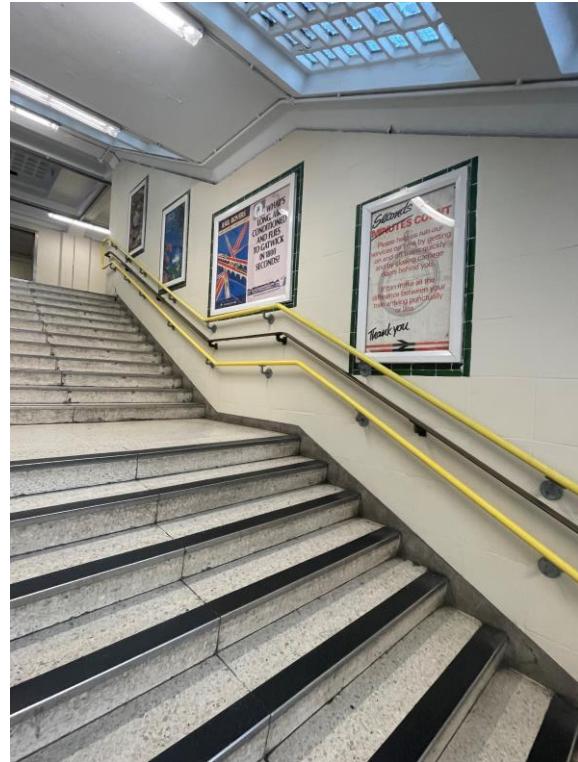


HANDRAIL DESIGN INSPIRED BY THE SOUTHERN RAILWAY

Elephant & Castle Station – detail studies



Richmond Station – Southern Railway Architects 1935 – Restored front bronze canopy



Elephant & Castle Station
Proposed Front (East) Elevation
I:150 @ A3



0 1 2 3 4 5 10 METERS

Elephant & Castle Station

DESIGN STUDY

Copyright Benedict O'Looney Architects 2021. Do not scale from these drawings.
Verify all dimensions on site and report any discrepancies to the architect / contract administrator immediately.
All structural elements to be designed by the structural engineer.

SCALE: I:150 @ A3
I:75 @ A1

DRAWING: EAC_p_150_e1
DATE: 27/03/23

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FINDLATER'S



BOROUGH HIGH STREET



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EXPRESS
DAIRY
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EXPRESS
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AFTERNOON
TEAS
LUNCHEONS
SMOKING ROOM

AFTERNOON
TEAS
LUNCHEONS
LADIES ROOM



EARLY AFTERNOON

TEAS

LUNCHEONS

LADIES ROOM